

APPROVED

Planning & Development Services

Manager, Urban Design

Date

Urban Design Brief Proposed Mixed Use Development

OAKVILLE GARDEN RESIDENCES CORP.

109 Garden Drive Oakville, ON

City File No.: SP.1320.007/01



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OAKVILLE

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Disclaimer:

The text and images contained in this document are only a conceptual representation of the intended version and character of this development. In this regard, they should not be construed or interpreted literally as to what will be constructed. Although this design brief represents Town standards for various design elements at the time of issue, final design may vary from standards shown in this brief as standards in technology, may change.

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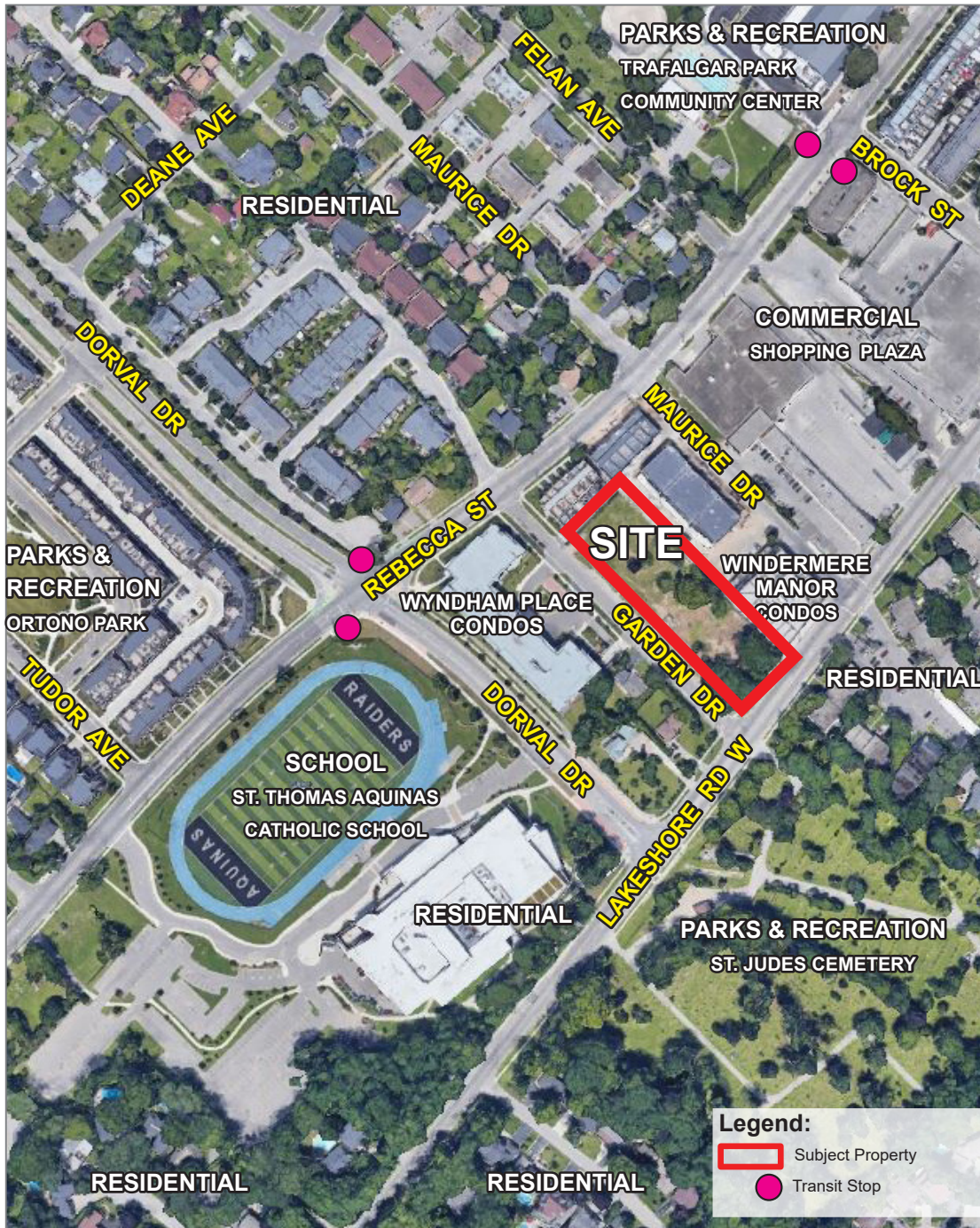


Figure 1: Area Context

Scale: NTS 

1.1 Context

The “Subject Lands” or “Lands” are located northeast of the Lakeshore Road West and Garden Drive intersection. They have an area of approximately ~4,988 metres squared (0.50 ha) with an approximate frontage of ~133 metres onto Garden Drive and approximately ~38.5 metres onto Lakeshore Road West. The Lands are currently vacant and as such do not contain any existing buildings or structures. From a grading perspective, the Subject Lands slope downwards from the eastern property line towards Garden Drive; as well as from the northern property line towards Lakeshore Road West. As such, the Subject Lands generally sit lower than the adjacent lands to the north and east.

The site will include one block of townhouses with ground level retail fronting Lakeshore Road West and three blocks of 3-1/2 storey back-to-back townhouses. Covered parking will be provided at ground level for each building. The site will also include associated surface retail and visitor parking spaces, ground level amenity areas, landscape buffers, and pedestrian walkways.

1.2 Surrounding Context

The Subject Lands are surrounded by a mix of residential and commercial uses. Surrounding land uses include:

- North: Immediately north of the Subject Lands are three-storey townhouses fronting Rebecca Street and accessed by a rear laneway from Garden Drive. Further north, across Rebecca Street, there are single detached dwellings, as well as front and rear loaded condominium townhouses.
- East: Immediately south/east of the Subject Lands is a four-storey condominium apartment building that fronts onto both Lakeshore Road West and Maurice Drive with commercial uses at street level. Immediately north/east are three-storey condominium townhouse dwellings that front onto both Rebecca Street and Maurice Drive.
- South: Immediately south of the Subject Lands is Lakeshore Road West. South of Lakeshore are two-storey townhouse dwellings that front onto Lakeshore, as well as the St. Jude Cemetery.
- West: Immediately west of the Subject Lands is Garden Drive. West of Garden Drive is a four-storey apartment building as well as two single-detached dwellings. Further west of the Subject Site, on the south side of Rebecca Street, is St. Thomas Aquinas Catholic High School and an associated sports field. Northwest of Rebecca Street/Dorval Drive are three-storey rear loaded townhouses and two-storey detached dwellings.

1.3 Opportunities & Constraints

The site design process has presented a set of opportunities and constraints related to the proposed development. Opportunities and Constraints are as follows:

1.3.1 Opportunities:

- Lakeshore Road West is a major town-wide transportation corridor. Existing transit stops along Lakeshore Road West, located within close proximity of the site, will provide convenient access to the Town's transit system.

- Located less than 100m east of Dorval Drive, convenient access is afforded to Highway 403.
- The high traffic volumes along Lakeshore Road West will provide the ground-level commercial high visibility and accessibility.
- The proposed increase in density on the subject property, located approximately 1km west of Downtown Oakville and Kerr Village, is supportive of the Town's development principles. The increased density of this infill development will support the use of public transit and viability of local retail, services, and amenities.
- Existing commercial land uses located along Lakeshore Road West to the east of the development will provide convenient access to retail and services within walking distance for community residents.
- St. Thomas Aquinas Secondary School is located 100m to the west of the site.
- The site is located approximately 350m from Lake Ontario, affording connections within walking distance to the network of parks and trails located along the lakeshore.

1.3.2 Constraints:

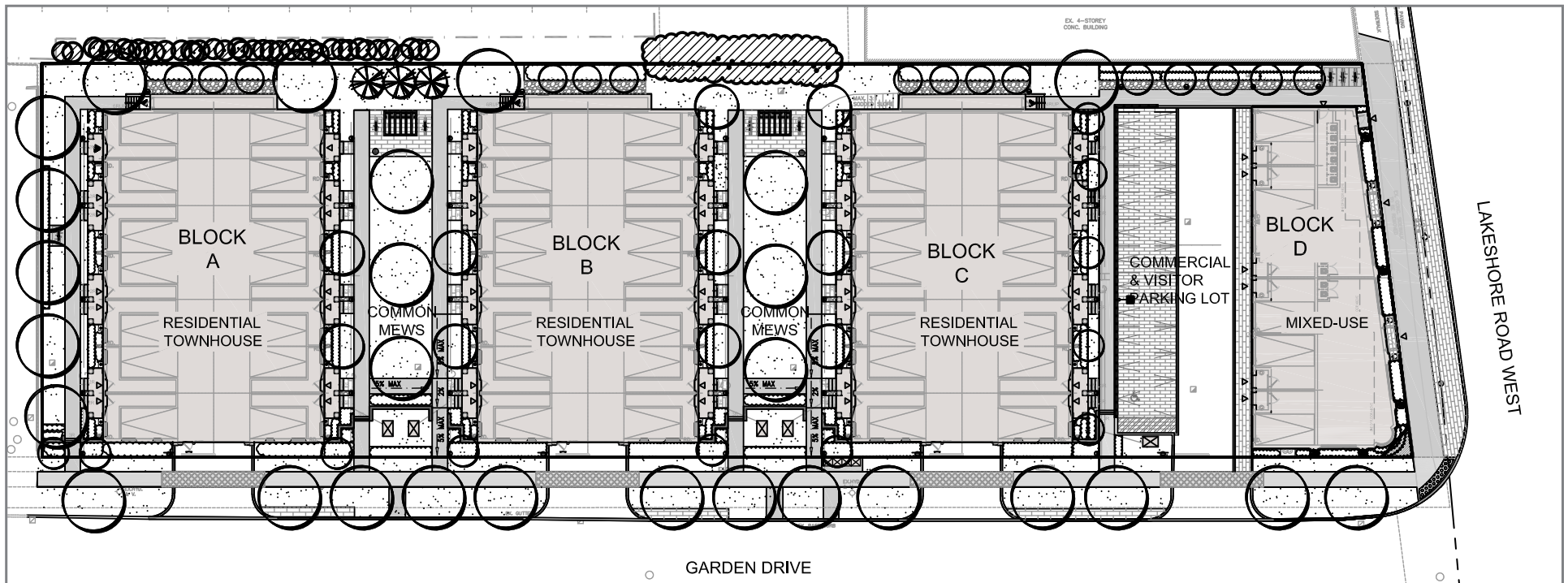
- Lakeshore Road West is a major road which supports high traffic volumes. Vehicular activity along Lakeshore Road West will require buffering from the residential units located in the proposed mixed-use development.
- Site access is provided exclusively along Garden Drive.
- Built form requires transition from the existing residential located adjacent to the site.
- Existing overhead hydro lines with low-level wires will preclude the provision of boulevard trees along the Lakeshore Road West boulevard.

1.4 Development Concept

The development concept consists of three back-to-back townhouse blocks and one mixed use block, generating a total of 48 residential units and a commercial Floor Area of ~241.0m². Both the townhouse blocks as well as the mixed-use block will be four storeys in height. The primary pedestrian entranceways to the three townhouse blocks will be from common landscaped mews which divide the blocks, whereas the primary vehicular access to each block will be from Garden Drive. The mixed-use block will provide for grade related pedestrian access to the commercial space from Lakeshore Road West as well as rear access from a common lane for the residential units and their associated garages.

48 parking spaces will be provided for residents in at-grade garages (1 per unit). Resident parking for each back-to-back townhouse block will be located in shared at-grade garages accessible from Garden Drive. The driveway accesses on Garden Drive are setback 7.36 metres from the proposed front of curb which leaves sufficient room for a car to idle in the driveway while entering the garage to avoid queuing on Garden Drive while the garage doors are opening. Resident parking for the mixed-use block will be located on the north side of the mixed-use block in private garages. 11 visitor/commercial parking spaces are provided north of the mixed-use block. In total, there will be four vehicular access points to the development from Garden Drive.

The common landscaped mews between townhouse Blocks A, B, and C, will provide outdoor amenity areas for the residents. Walkways in the common mews provide pedestrian connection to the front entries of the residential units facing on to the open space. Walkways will provide pedestrian connections throughout the site and to the municipal sidewalks along Lakeshore Road West and Garden Drive. An accessible walkway connection will be provided to each mews area.



1.5 Planning Justification

Halton Region Official Plan

The Subject Lands are located within the Urban Area and Built Boundary on Schedule 1h of the Region of Halton Official Plan (HROP). Lakeshore Road West is classified as a Minor Arterial Road. Further, the Lands are located within a Strategic Growth Area as defined by the HROP.

As is identified below, the Lands are located within a Nodes and Corridors designation in the Oakville Official Plan and more specifically, located within the Kerr Village Growth Area. Accordingly, the Lands are located within a Strategic Growth Area per the HROP.

Per Policy 51, Urban Areas identified by the HROP are where urban services are provided to accommodate concentrations of existing and future development. To support forecasted population growth, the HROP provides intensification and density targets.

Section 3 of the HROP outlines specific directions pertaining to land use planning and the land use designations within the HROP plan area. Goals set out by the HROP for designated Urban Areas include fostering complete communities and improving housing affordability. Accordingly, Policy 72.1 states that objectives for Urban Areas include (among others):

- Supporting a form of growth that is compact and supportive of public transit and non-motorized modes of transportation (2);
- Providing a range of complete communities of various sizes, types and characters (3);
- Directing growth to Strategic Growth Areas (6);
- Facilitating and promoting intensification and increased densities (9); and,
- Providing for an appropriate range and balance of retail uses (among others) to meet long-term needs (10).

Further to the objectives outlined above, Policy 78.1 and 79 provide specific objectives for Strategic Growth Areas (SGAs). Policy 78.1(2) directs that a significant proportion of population should be focused within SGAs through mixed use intensification. Policy 79 provides the following additional objectives

(among others):

1. An urban form that promotes the development of complete communities;
2. Opportunities for more cost-efficient and innovative urban design;
3. A diverse and compatible mix of land uses;
4. A pedestrian-oriented urban environment;
5. Attract a significant portion of population and employment growth;
6. Achieve higher densities than the surrounding areas; and,
7. Achieve an appropriate transition of built form to adjacent areas.

Regarding the provision of housing, the Policy 84 of the HROP states that “[t]he goal for housing is to supply the people of Halton with an adequate mix and variety of housing to satisfy differing physical, social and economic needs”.

The proposal conforms to the policies listed above. The proposed townhouse units will contribute to intensification within a settlement area, the built boundary and a SGA. The mix of uses provided there-on will contribute to creation of a complete community and, when combined with the proposed compact built form, will support alternative modes of transportation. Further, the proposed back-to-back townhouse built-form will add to the diversification of the Town of Oakville’s housing supply, which in turn will provide housing options that are smaller in size and more attainable. Further to this, the proposed development will work towards policy 86.6 of HROP that requires that the development of 65 percent of new housing consist of townhouses and multi-storey buildings.

Town of Oakville Official Plan

Schedule A1 of the Oakville Official Plan (“Oakville OP”) designates the Lands Nodes and Corridors as well as Main Street Area. Further, the Lands are within a Growth Area per Schedule G and are more specifically identified as being within the Kerr Village Growth Area. On Schedule O1 the Lands are designated as Main Street 1. Schedule O1 also identifies the lands as being subject to exemption policies found in Part E of the Oakville OP. Lastly, Lakeshore Road West is designated as a Primary Street per Schedule O2.

The Urban Structure policies for the Town are provided in Part C, Section 3 of the Oakville OP. This section provides a description of each element of the Town's Urban Structure, including Nodes and Corridors in Policy 3.6. This Policy indicates that these parts of the Town are identified as the focus for mixed use development and intensification and are SGAs as defined in the Growth Plan. This policy also states that each Node has a unique existing and planned character, scale, and potential to accommodate growth. Kerr Village is specifically intended to develop as a mixed-use centre with viable main streets. Section 4 of the Oakville OP builds on Section 3 and provides policies to manage growth and change. Policy C-4.1 notes that Growth Areas are where the majority of growth and intensification is intended to occur.

A general objective of the Oakville OP Design policies is to provide for “innovative and diverse urban form and excellence in architectural design” (6.1.1(c)). Policies pertaining specifically to streetscapes are identified by Policy 6.4, setting out provisions that streetscapes shall enhance the local context and create a sense of identity; promote a safe, attractive, and barrier-free pedestrian environment; provide well designed and coordinated tree planting, landscaping, lighting and furnishings; provide wayfinding and navigational information, and provide cohesion and seamless transitions between the public and private realm (6.4.1). New developments are to contribute to the creation of a cohesive streetscape by placing the principal building entrances towards the street, framing the street, providing variation in façade articulation, and connecting active uses to the public realm. Above-ground utilities should be grouped to minimize visual and physical intrusions on. Policy 6.9 directs that the built form of new development should foster a sense of place; be compatible with the existing and planned surrounding context through landscape buffering, spatial separation, and compatible built form; present active and visually permeable facades; have principal entrances that front onto the street; and discourage continuous street walls and building height.

Policy 6.10 directs that landscaping design and treatments create an attractive and human scaled environment for pedestrian, define and demarcate various functions within a development, increase tree canopy coverage, integrating a diverse mix of native plant species, and provide shade and wind protection. Policy 6.11 directs that pedestrian walkways should be barrier-free, safe and direct. Policy 6.12 directs for safe and direct vehicular access and circulation routes as well as consolidation of driveway accesses to maximize the areas available for landscaping and minimize disruption of the public sidewalk. Policy 6.13 states that surface parking areas should be located in the rear or side yard and appropriately screened so they do not dominate the streetscape.

LEGEND

-  NATURAL HERITAGE SYSTEM
-  PARKWAY BELT
-  GREENBELT
-  NODES AND CORRIDORS
-  EMPLOYMENT AREAS
-  MAJOR COMMERCIAL AREAS
-  RESIDENTIAL AREAS
-  MAJOR TRANSPORTATION CORRIDOR
-  PROPOSED MAJOR TRANSPORTATION CORRIDOR
-  MAJOR TRANSIT STATION
-  RAILWAY LINE
-  UTILITY CORRIDOR

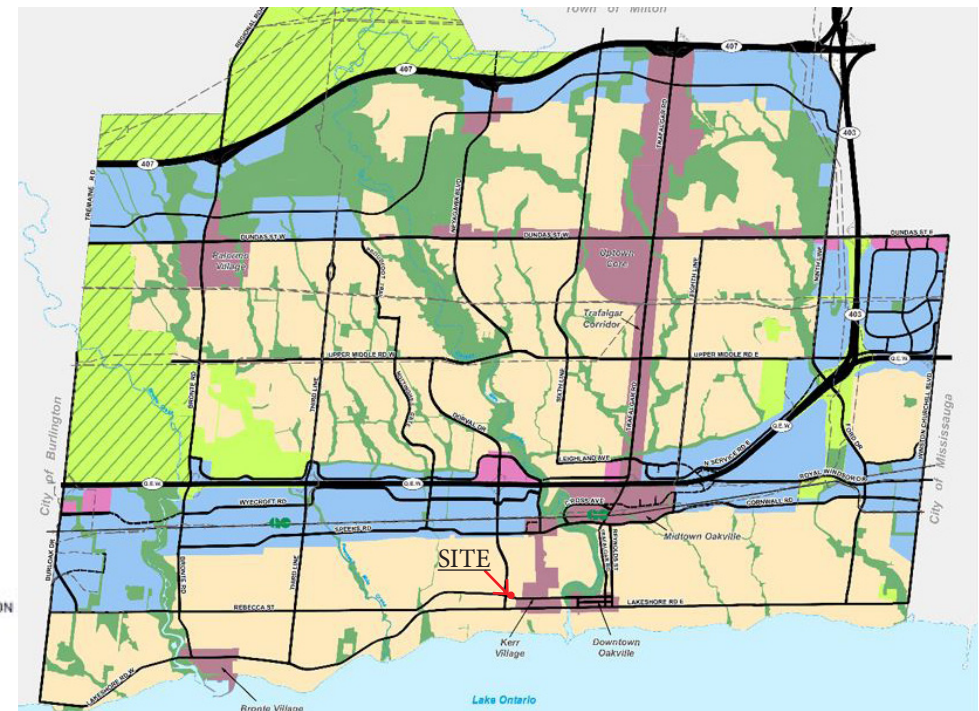


Figure 3: Schedual A1 Urban Structure

Part D of the Oakville OP provides policies associated with Land Uses.

Policy 12.1 notes that the intent of the Mixed-Use designation is to allow for a diversity of residential, commercial and office uses in integrated buildings in Growth Areas and along identified corridors. Development in these designations is intended to create animated streets, with the location and size of any use on upper and/or lower floors determined through the development process.

Policy 12.2 provides policies for Main Street 1 lands. Permitted uses within the Main Street 1 designation can include a range of retail and service commercial uses as well as residential uses. That noted, retail and service commercial uses are to be primarily located at the ground floor of the building (Policy 12.2.1). Within the Main Street 1 designation, buildings are to be a minimum of two storeys and a maximum of four storeys in height (12.2.2) and surface parking is to be located only within a side and/or rear yard (12.2.3).

Part E of the Oakville OP provides policies for specific Growth Areas, and policies for Kerr Village are found in Section 23. Policy 23.2 provides objectives for development in Kerr Village including compact, high density and compatible development (policy 23.2.1). A further objective is creating an attractive public realm by promoting high quality streetscapes and open spaces as well as ensuring appropriate transitions occur between the main street areas and lower density areas (policy 23.2.3).

Section 23.4 provides transportation related policies and notes that through the development process surface parking should be minimized and other TDM measures should be implemented to encourage transit ridership (policy 23.4.1b). Access to parking and servicing areas should be provided from local streets at the side or rear of buildings (policy 23.4.1c). Bicycle facilities are encouraged throughout Kerr Village such as bicycle racks and bicycle lockers (policy 23.4.1d). Policy 23.5.3 directs that Primary Streets shall provide for pedestrian-oriented streetscapes through the use of wide sidewalks, landscaping and furnishings; incorporate a high degree of transparency on the ground floor; provide building openings and principal entrances facing the street; and, contain commercial uses (among others) adjacent to the street (policy 23.5.3a&b).

The proposed development conforms to the general intent of the Oakville OP by providing for mixed use development and intensification in an identified Growth Area. However, an Official Plan Amendment is required as the site-specific policies applying to the Lands per Policy 23.7.6 are very prescriptive and only allow for two very specific development options. With respect to the proposed density and height of the development the 48 units proposed in a 4-storey built form represents appropriate intensification for a Growth Area in the Town of Oakville and falls within the ranges established by previous approvals on the Subject Lands. Importantly, the proposal includes a mix of uses, with commercial uses facing Lakeshore to animate the streetscape/ Primary Street as directed by the Main Street 1 land use policies. Further, the building locates parking and access from a local street and within a side and/or rear yard. Through the Site Plan process more specific design elements associated with the Lakeshore Road West frontage including provision of wide sidewalks, landscaping and furnishings as well as a high degree of transparency can be secured and achieved.

With regards to the development's integration and transition with adjacent lands and uses, the development's proposed height and massing are appropriately scaled to the immediate context. The residential uses to the north/east of the Subject Lands are townhouse dwellings that are 3-storeys in height. To the immediate west and south/east of the Subject Lands, there are 4-storey apartment buildings. At 4-storeys, the proposed development will be consistent with the heights of adjacent buildings, and the block lengths will be comparable to the townhouse block located to the immediate east. Additionally, the grading of the Subject Lands is significantly lower than the adjacent lands to the east and north, which will result in the height of the proposed development being less apparent relative to the surrounding 3-storey townhouse buildings - reducing overlook/privacy and shadowing concerns. Further, along the northern and eastern lot lines significant landscape strips are proposed (ranging between 4.5m to 6.2m) which will allow for provision of significant landscape buffers. Where buildings are closer to the lot line (4.5m to the eastern lot line) only secondary windows will be proposed to further reduce overlook.

The principal entrances of the townhouse units will front onto private mews rather than Garden Drive. This design will foster greater opportunity for interconnection amongst neighbouring residents, as these will be pedestrian-oriented spaces, and because the buildings will be oriented towards one another on the shared pathway. However, these access points will remain visually connected to Garden Drive due to the 15m of building separation provided. The introduction of additional specific design elements is possible through the Site Plan process and will be explored to further mitigate any potential transitional concerns.



Figure 4: Example of Private Mews

1.5.1 Livable by Design Manual

The Livable by Design Manual (LBDM) provides urban design direction for the Town of Oakville. The Manual is intended to implement the Town’s urban design policies as are outlined in Section 6 of the Oakville OP.

Part A of the Livable by Design manual provides urban design direction for all forms of development, redevelopment and capital projects and will be the focus of this review. Parts B is not applicable to the proposed development. Part C provides specific guidance for site design and will be used during the site plan phase.

General objectives set out by the LBDM are carried over from the Oakville OP and are the following:

- To provide diversity, amenity, comfort, safety and compatibility with the existing community.

- To encourage attractive, safe public spaces, such as streetscapes, gateways, vistas and open spaces.
- To promote innovative and diverse urban form and excellence in architectural design.
- To promote the creation of distinctive places and locales, including Growth Areas.

1.5.1.1 Complete Streets

Section 2.1 of the LMDB outlines guidelines for the development of complete streets. These guidelines will be used to design the street fronts of both Garden Drive and Lakeshore Road West.

- Create ‘Complete Streets’ that are multi-modal with convenient connections and which prioritize pedestrian and cyclist comfort.
- Maintain continuous street wall by orienting primary building facades and entranceways directly towards the street.
- Plantings shall provide buffers between pedestrian pathways and roadways.

1.5.1.2 Low-rise Non-residential Buildings

Section 3.2 of the LBMD provides design direction for low-rise non-residential developments. These guidelines are applicable to mixed use developments and as such apply to the at grade commercial component of the proposed development.

- Orient and position buildings such that they create an active pedestrian streetscape.
- Buildings to be compatible with the existing and planned surrounding community context.
- Maximize public windows and door openings along the public realm.
- Minimize visibility of surface parking.
- Promote pedestrian scaled environments with modulation, projects, recesses, pronounced vertical elements, corner and parapet features, and distinct rooflines.
- Extend building facades along property lines that face the public realm.

- Corner buildings to accentuate corner and address both street frontages.
- Principal building entrances should be barrier free, obvious, located at grade and face public realm.
- Buildings to reflect a high standard of design and detail.
- Mechanical elements should be screened from view.

1.5.2.1 Low-rise Residential Buildings

The following policies from the LBDM guide the development of low-rise residential buildings. These policies are set out in Section 3.7 and apply to buildings that are primarily medium- and high-density, however are a maximum of five storeys in height.

- Orient buildings toward the primary public street to foster an active pedestrian environment along the street edge.
- The height and massing of buildings should be designed to create and reinforce a pedestrian-scaled environment
- Facades should be designed such that they incorporate a high level of architectural treatment, which can include varied wall planes and roof lines, human scale portions, large windows, and porches/entranceways
- Incorporate architectural variety between adjacent blocks and along streetscapes
- Main entranceways should be oriented towards the public realm and designed such that is accessible, illuminated and has weather protection
- Buildings should be designed with a high standard of design, detail and variety of materials.
- Vehicle accesses and parking should be located internal to the site or at the rear of the side of the building.
- Environmental controls should be integrated throughout the building design to regulate sun and wind exposure
- Utility features such as drainage pipes, vents and metres should be thoughtfully integrated into the building design and screened from view in the public realm
- Bird friendly facades should be incorporated

- The roof should be designed as an overall compliment to the building design and should incorporate a variety of roof line elements and features to increase visual interest and minimize the massing
- Rooftop mechanical equipment should be incorporated directly into the structure and design of the roof to minimize visual impact

1.5.2.2 Landscape

- Landscape shall be provided to enhance the pedestrian environment and soften the built form.
- Provide tree planting throughout the site and within the public realm to contribute to the Town's target of increased canopy coverage.
- Provide a diversity of plant species.
- Specify drought and salt tolerant plant species that are hardy to site conditions.
- Specify plant species that are resistant to pests and disease.
- Provide landscaping that will enhance pedestrian connections, building facades, and built-form.
- Screen views of utilities, loading, servicing, and waste management areas with appropriate landscape screening.

1.5.2.3 Pedestrian Connections

- Provide convenient pedestrian connections to the public realm.
- Variations in paving colour and texture to emphasize pedestrian areas, entrances, vehicular routes, etc.
- The location of shade trees and site furnishings will contribute to a comfortable pedestrian environment.

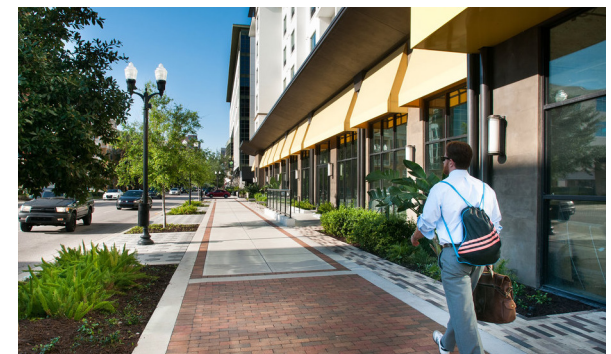


Figure 5: Example of Variations of Paving to Emphasize Pedestrian Areas

1.6 Linkages, Connections & Circulation

1.6.1 Vehicular Circulation

- Covered parking is provided at ground-level for residential townhouse blocks A, B, and C, with vehicular access provided from Garden Drive.
- Covered parking is provided at ground-level for the residential units in Block D. Vehicular access to the covered parking is provided from the parking lot at the rear of the building.
- Retail, accessible, and visitor parking spaces are provided in the outdoor surface parking lot located on the north side of Block D. Vehicular access to the parking lot is provided on Garden Drive.
- Vehicular circulation routes shall be clearly defined, convenient and safe.

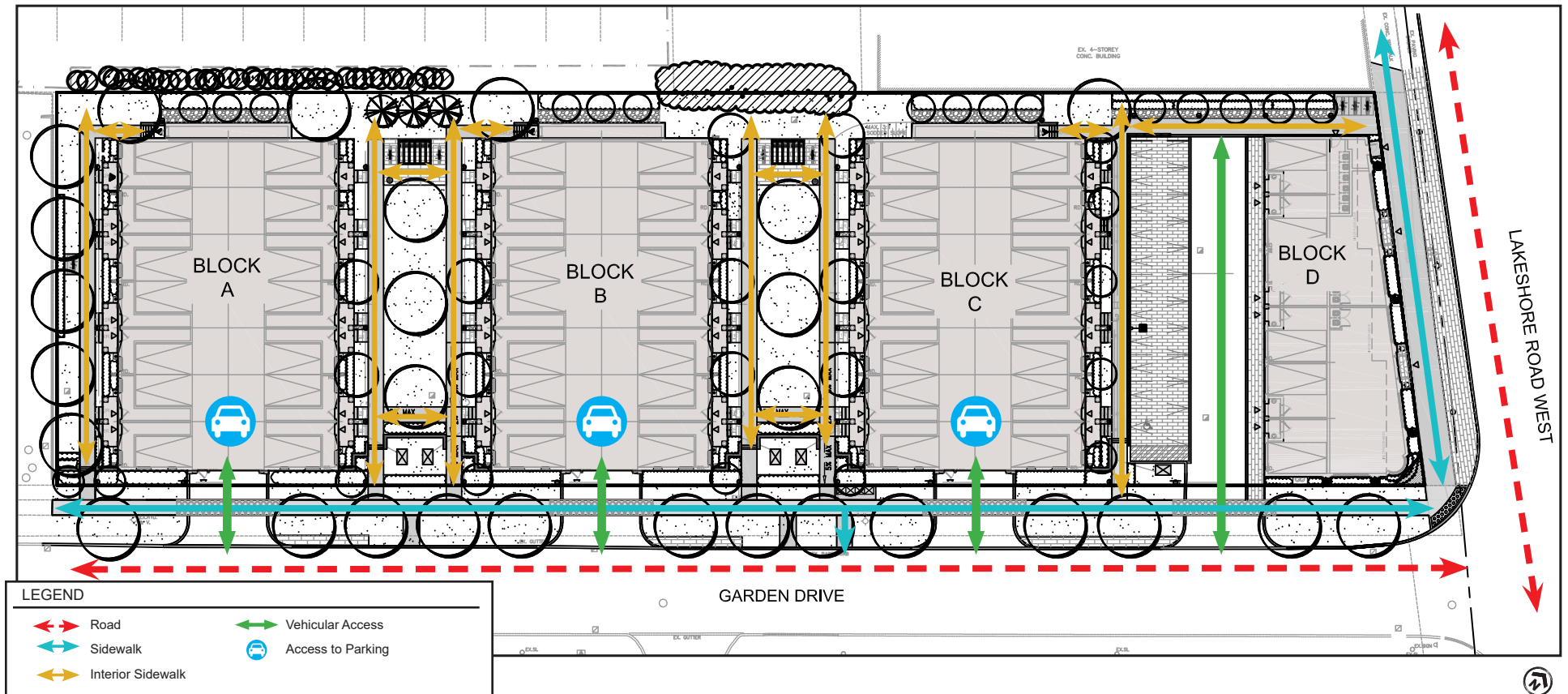


Figure 6: Pedestrian and Vehicular Movement Map

1.6.2 Pedestrian Circulation

- Pedestrian walkways will be provided to connect building entrances, parking areas, and municipal sidewalks.
- Unit paving along the frontage of Block D will provide pedestrian access between the entries to the ground-level retail units and the 3.0m wide sidewalk along Lakeshore Road West.
- Walkway connections are provided between the private entrances to the residential units and 1.5m wide walkways located along the perimeter of the mews. The 1.5m walkways provide pedestrian routes which will connect to the 1.5m wide municipal sidewalk along Garden Drive.
- Walkway connections to the sidewalk along Garden Drive are provided for the residential units at the ends of Blocks A, B, and C.
- Pedestrian walkways shall be clearly defined to avoid unsafe conflicts with vehicular circulation. The incorporation of decorative and contrasting paving, plantings, site furnishings, and built form shall emphasize and delineate pedestrian areas and walkways.
- CPTED principles and guidelines shall be incorporated into the design of walkways to provide a safe pedestrian environment.
- Accessible parking spaces shall be located near building entrances.
- Deciduous canopy trees located along walkways will provide shade for pedestrian circulation routes and seating areas.
- The design of pedestrian walkways and seating areas shall incorporate screening for buffering of wind and protection from the elements.
- Lighting shall be provided for walkways and pedestrian areas for visibility and safety in accordance with CPTED guidelines.

1.6.3 Active Transportation

- The Town of Oakville's Active Transportation Master Plan encourages the use of active transportation within communities. Pedestrian walkways located within the proposed development will provide convenient pedestrian access to services and amenities for residents and visitors.
- Provision of adequate, convenient bicycle parking encourages travel by bicycle for purposes other than recreation. As encouraged by The Town's Active Transportation Master Plan, bicycles will be viable active transportation options for commuting, shopping, and entertainment trips.
- Bike parking is provided on the east side Block D, with convenient access between the 3.0m wide sidewalk and the ground-level retail along Lakeshore Road West.
- Short-term bike parking spaces are provided adjacent to seating areas in the common outdoor mews.

1.6.4 TRANSIT

- Oakville Transit operates transit routes along Rebecca Street, within walking distance of the proposed development.
- The Oakville Transit routes along Rebecca Street provide convenient connections to GO Transit service routes operating north of the site.

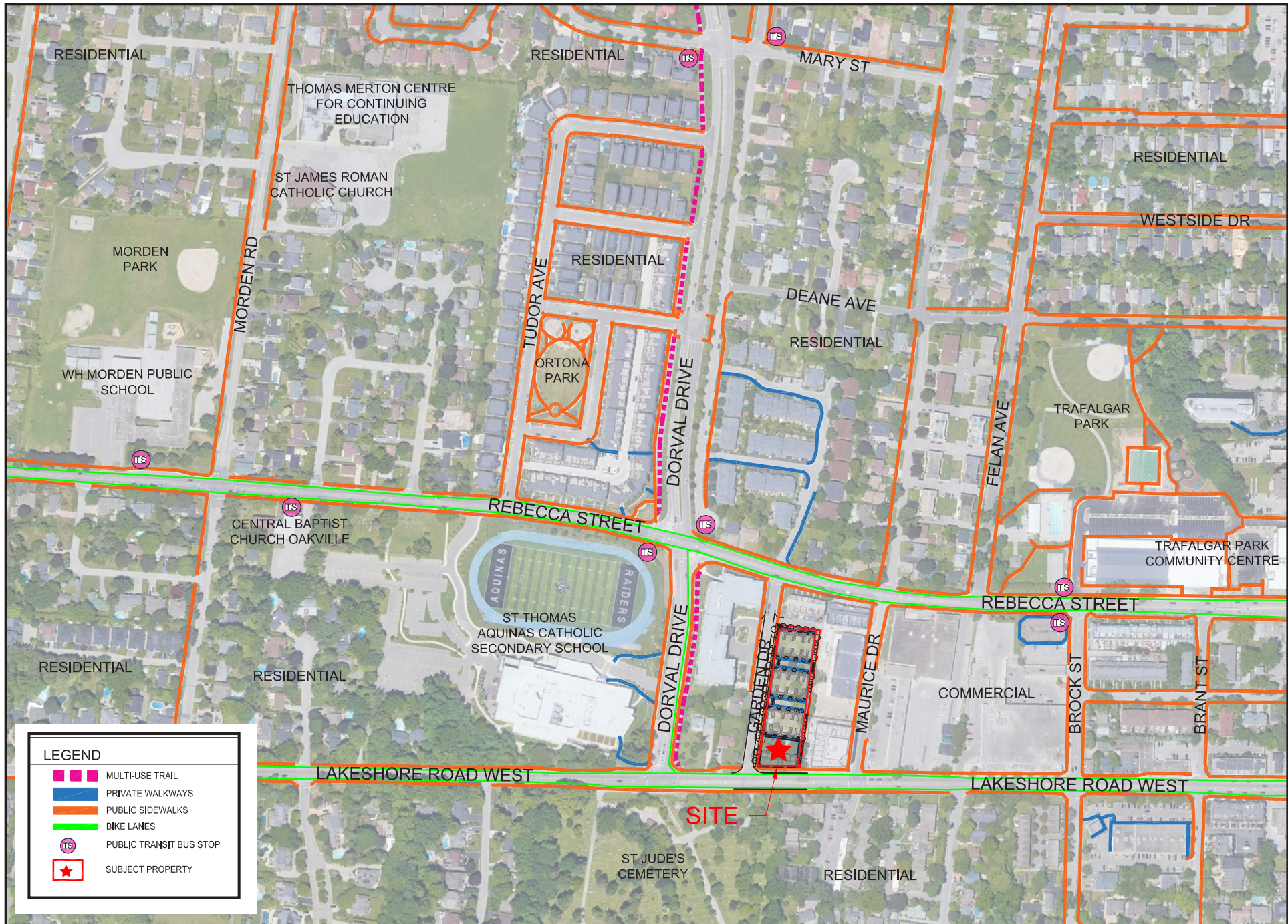


Figure 7: Pedestrian Movement Map Surrounding the Site

2. Landscape Guidelines

The purpose of this chapter of the Design Brief is to describe and illustrate the design intent of the proposed landscape treatments for the proposed residential development.

Figures and images are provided to show the intended character of the proposed landscape design. Final design solutions will be resolved during Site Plan Approval and the detailed design stage of the project in consultation with the Owner, the Town of Oakville, and the team consultants. Site landscaping will be in accordance with the Town of Oakville's Site Plan requirements.



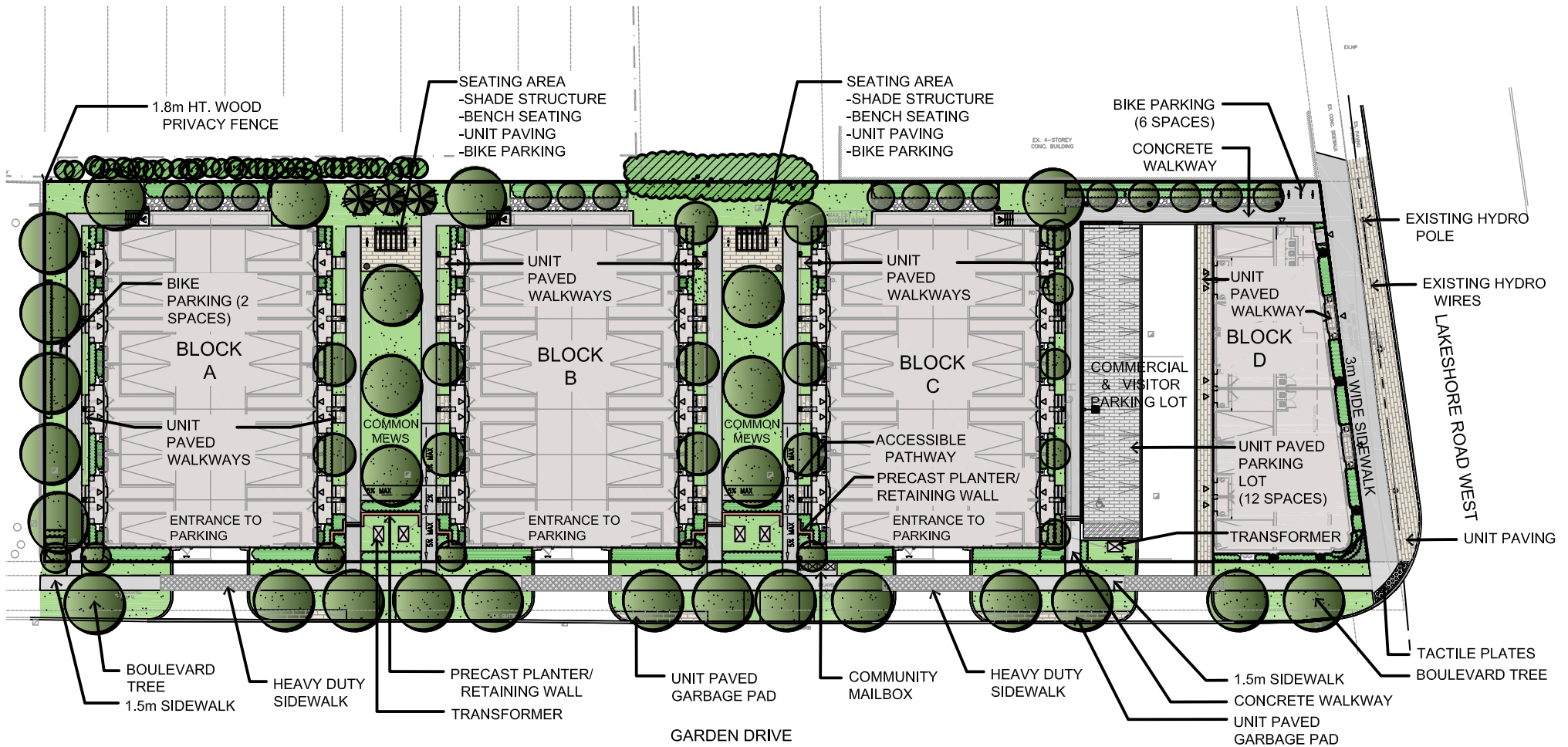


Figure 8: Landscape Master Plan

2.1 Landscape Elements

2.1.1 Tree Canopy Cover

The Town of Oakville is committed to preserving and enhancing its tree canopy, recognizing the environmental, economic, and social benefits offered by healthy tree growth within a community. In compliance with the Livable Oakville Plan, the proposed development will contribute towards the Town's urban forest and town-wide 40% canopy coverage target.

In accordance with Town requirements, a Canopy Cover Plan and calculation chart will be provided as part of the Site Plan Application. The Canopy Cover Plan and accompanying chart will demonstrate how the current and projected tree canopy for the development will meet the Town's canopy cover targets.

The Town has prescribed the following minimum canopy coverage targets by land use area:

- Residential land use 20%
- Arterial Roads and Collector Roads 34%

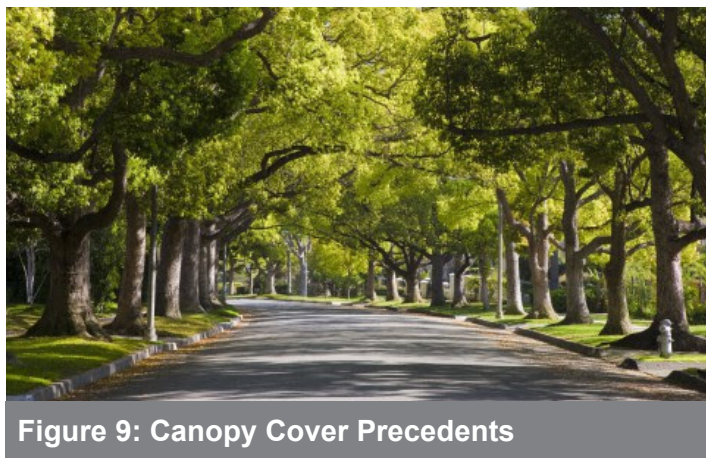


Figure 9: Canopy Cover Precedents

The following strategy will be implemented throughout the development site to ensure the required canopy coverage targets are achieved:

- Large stature trees (14m + spread) will be planted within the boulevard of Garden Drive at an average spacing of 8-10m where spacing between utilities allows.
- Boulevard trees cannot be planted along Lakshore Road East due to low overhead Hydro wires.
- Large stature trees will be planted within the site where adequate soil volume is available and space allows.
- Medium stature trees (10-13m spread) will be planted throughout the site in locations where space is constrained due to proximity to paved areas, utilities, built form, etc.

The Town's current tree planting and management practices provide direction to minimize the removals and potential damage to existing trees and to optimize growing conditions for new tree plantings. The implementation of the following best practices for the planting of new trees within the proposed development will ensure that targeted tree canopy coverage targets are achieved:

- Thirty cubic metres of soil is to be available for each new tree. This soil will allow for optimal root growth and absorption of nutrients, water, and air once the resources within the planted rootball have been exhausted.
- Fifteen cubic meters of soil may be provided for each tree in areas where soil volume is shared. Continuous soil trenches provided within boulevards provide opportunities for linkage

of soil areas which can be shared by multiple trees.

- Hardy trees with smaller canopy and rootball sizes shall be specified in planting areas where access to the required soil volumes cannot be provided. Smaller trees which are hardy to the growing conditions can thrive in reduced soil volumes resulting in sustainable canopy coverage.
- Tree species shall be selected for hardiness and resistance to pests and disease.
- A variety of tree species shall be specified to contribute to increased biodiversity within the development and surrounding community.
- The specification of hardy, native tree species is encouraged where feasible.
- Tree species should be selected which do not drop seeds or fruit for locations within proximity to barrier free pedestrian routes.
- Tree plantings shall conform with current Town of Oakville

standard planting details.

Tree species selected for the site may include the following:

- Sugar Maple*
- Bowhall Red Maple**
- London Plane Tree
- Hackberry*
- Tulip Tree*
- Princeton Sentry Ginkgo
- Ironwood*
- Allegheny Serviceberry*
- Chanticleer Pear
- Redbud*
- Pagoda Dogwood*
- White Spruce*

*Denotes native tree species.

**Denotes native cultivar tree species.

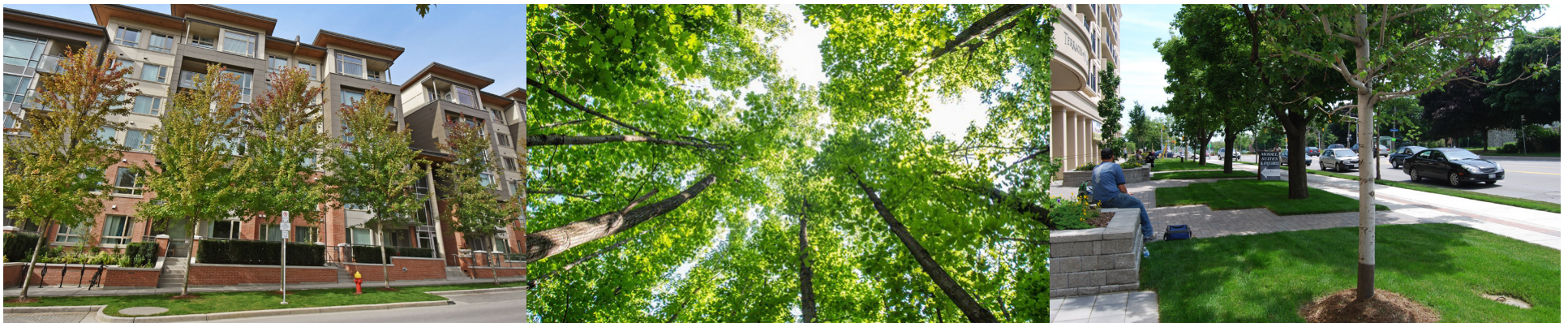


Figure 10: Canopy Cover Precedents

2.1.2 Boulevard Trees

- Boulevard trees are significant features of the streetscape and are subject to Town of Oakville requirements.
- Deciduous canopy trees shall be provided within the municipal boulevards of Lakeshore Road West and Garden Drive to provide shade for the pedestrian sidewalks, create visual interest, and to unify the community.
- Boulevard trees shall be planted where utilities allow and will be located as directed by the Town of Oakville's road profiles.
- Tree species should be in groups of 3-6 to provide species diversity.
- Narrow form trees will be required next to streetlights.
- Trees with mature height of 5m or less may be required where overhead wires are a concern.

2.1.3 Fencing

Wood Privacy Fence

- Existing privacy fence is located along the north and east property lines adjacent to existing residential.
- A section of new 1.8m high wood privacy fence is proposed along the east property to close the gap in the existing fence and provide privacy screening of the adjacent outdoor mews.
- The wood privacy fence will be constructed in accordance with the Town of Oakville's standards.

2.1.4 Decorative Paving

- Paving treatments shall be of high-quality materials that harmonize with the building's exterior architectural treatments.
- Decorative paving will be designed to define the various pedestrian zones and to highlight building entrances.
- High-albedo paving shall be provided for pedestrian areas and walkways where appropriate to reflect light and thereby mitigate the Urban Heat Island Effect.

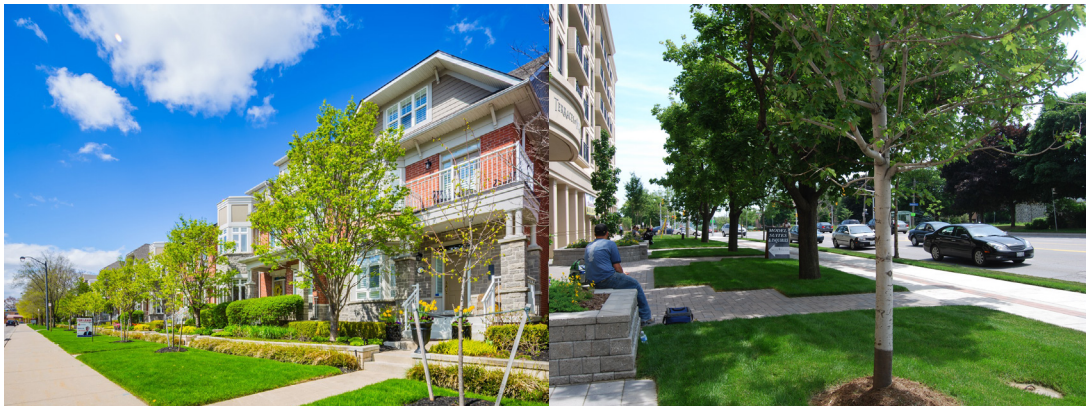


Figure 11: Boulevard Planting Example

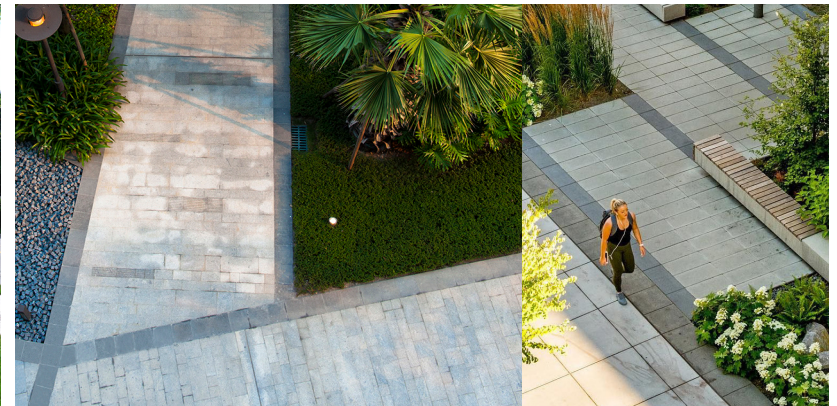


Figure 12: Decorative Paving Examples

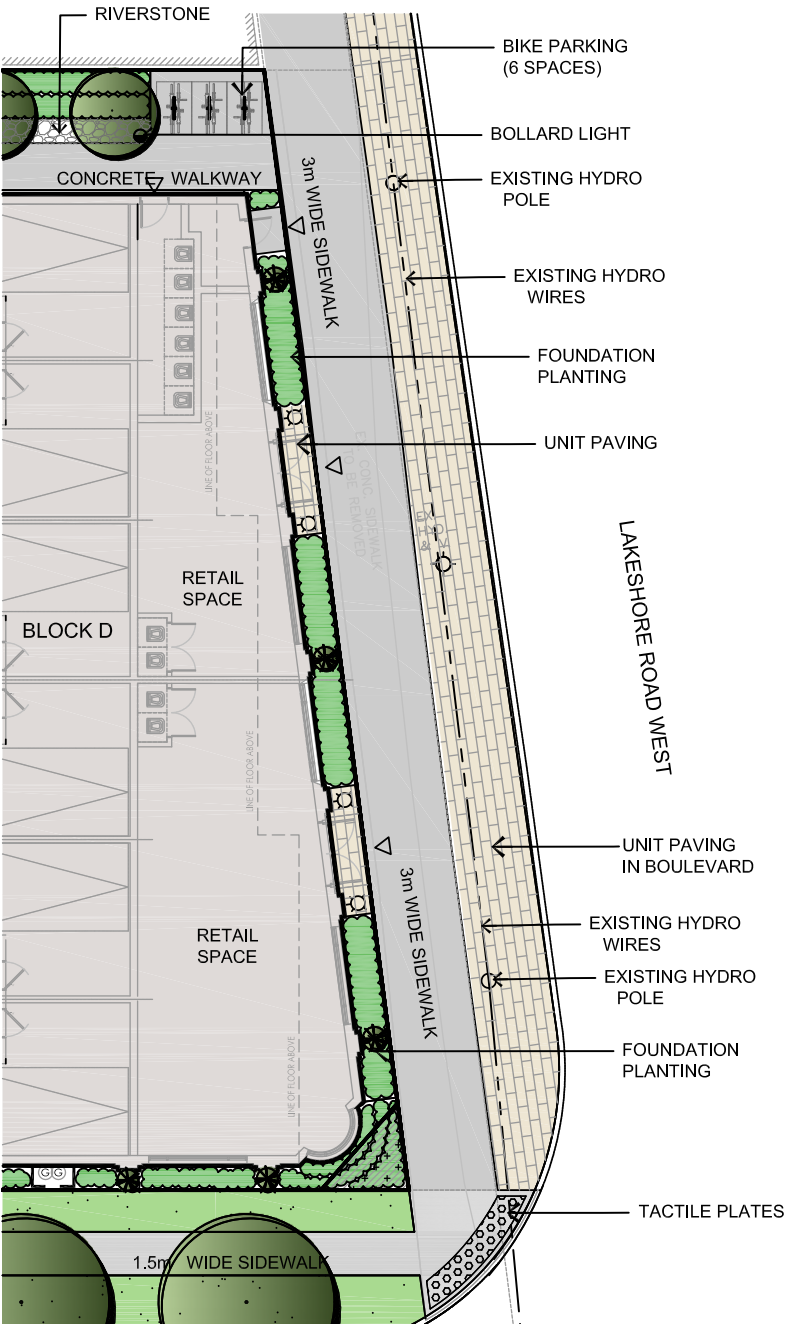


Figure 13: Lakeshore Road West Streetscape



2.2 Streetscape

Streetscape involves a combination of landscape elements including boulevard trees, ornamental planting, paving, and site furnishings which enhance the built form while delineating the boundaries between the public and private realms. Landscape elements contribute to a dynamic and vibrant streetscape and convey the character of the development while contributing to an enhanced pedestrian environment.

2.2.1 LAKESHORE ROAD WEST

- Lakeshore Road West, a major road and thoroughfare, forms the southern boundary of the site.
- The Lakeshore Road West streetscape includes a continuation of the existing streetscape to the east, which includes a 3m wide sidewalk and a unit paver boulevard.
- Decorative unit paving between the building and sidewalk provide an enhanced paving treatment along the frontage of the retail at entrances facing Lakeshore Road West. Pedestrian access to the retail entrances will be provided directly from the municipal sidewalk.
- Shrub planting beds will be provided along the building frontage between retail entrances.
- Bike parking is provided on the east side of Block D with convenient access to the 3.0m wide sidewalk and ground-level retail along Lakeshore Road West.



Figure 14: Example Retail Frontage

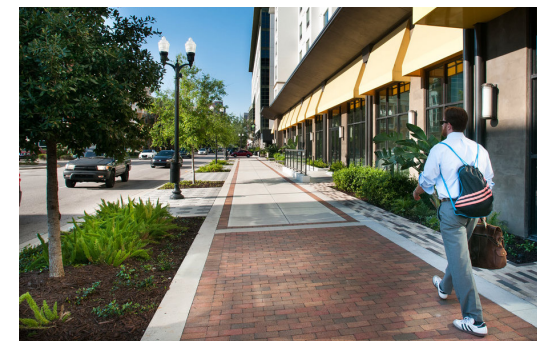


Figure 15: Example Retail Frontage

2.2.2 GARDEN DRIVE

- Garden Drive forms the western boundary of the site.
- Deciduous shade trees will be provided within the sodded boulevard. These trees will provide shade for the 1.5m wide municipal sidewalk located within the boulevard.
- The vehicular entrances to the covered ground-level parking areas for each building are located on Garden Drive. The continuous municipal sidewalk which will cross the vehicular driveways shall be constructed of contrasting paving materials for visibility and enhancement of pedestrian safety.
- The private front entrances to the end units of Blocks A, B, and C are located facing Garden Drive. Private walkways will provide pedestrian access from the residential units to the municipal sidewalk.
- Ornamental planting beds, including shrubs, perennials, and ornamental grasses provided along the building foundation facing the street, will enhance the building's architecture and soften the building façade.

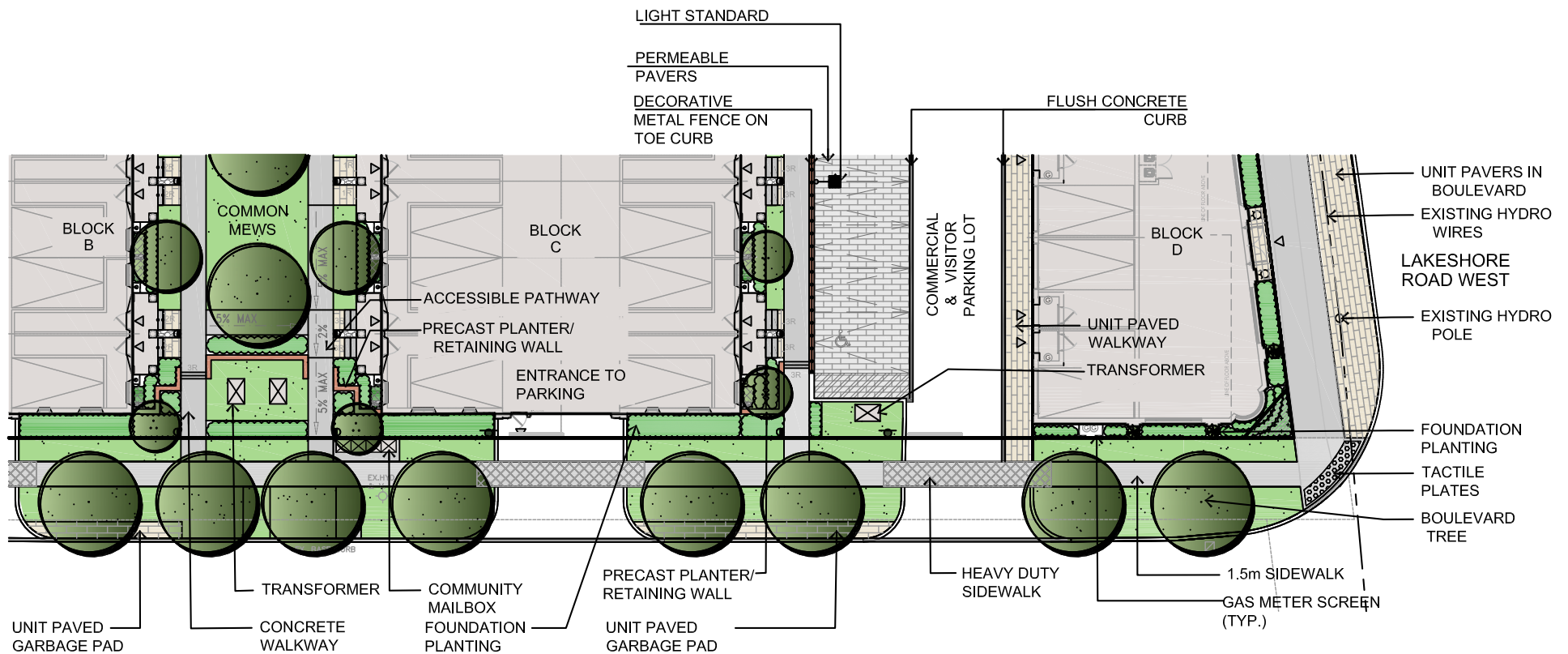


Figure 16: Garden Drive Streetscape

2.3 Outdoor Amenity Areas

Outdoor mews are located between townhouse Blocks A, B, and C. These amenity areas provide central common spaces which will provide opportunities for active and passive recreation, children to play, and residents to socialize.

- Front entrances of the residential townhouse units are oriented towards the outdoor mews areas. Private walkways connect the residential front doors to the walkways in the outdoor amenity areas.
- Walkways will provide pedestrian circulation routes through the site which will connect to the municipal sidewalk along Garden Drive.
- The outdoor amenity areas include seating areas with shade structures and benches.
- Open lawn areas provide flexible space for a variety of activities, including fitness, exercise, and children's play.
- Decorative unit paving, with colour and style selected to complement the built form, will define and enhance the spaces within the mews.
- The design of hard and soft landscape elements and features, including points of entry, will be consistent and complementary to the overall development.
- Landscape features within the amenity areas will complement the workmanship and architectural styling of the surrounding buildings and built form.
- Pedestrian entrances will be highly visible and accessed from defined sidewalks.
- An accessible (5% max. slope) walkway connection will be provided to each of the mews/ amenity areas.



Figure 17: Example of Outdoor Mews

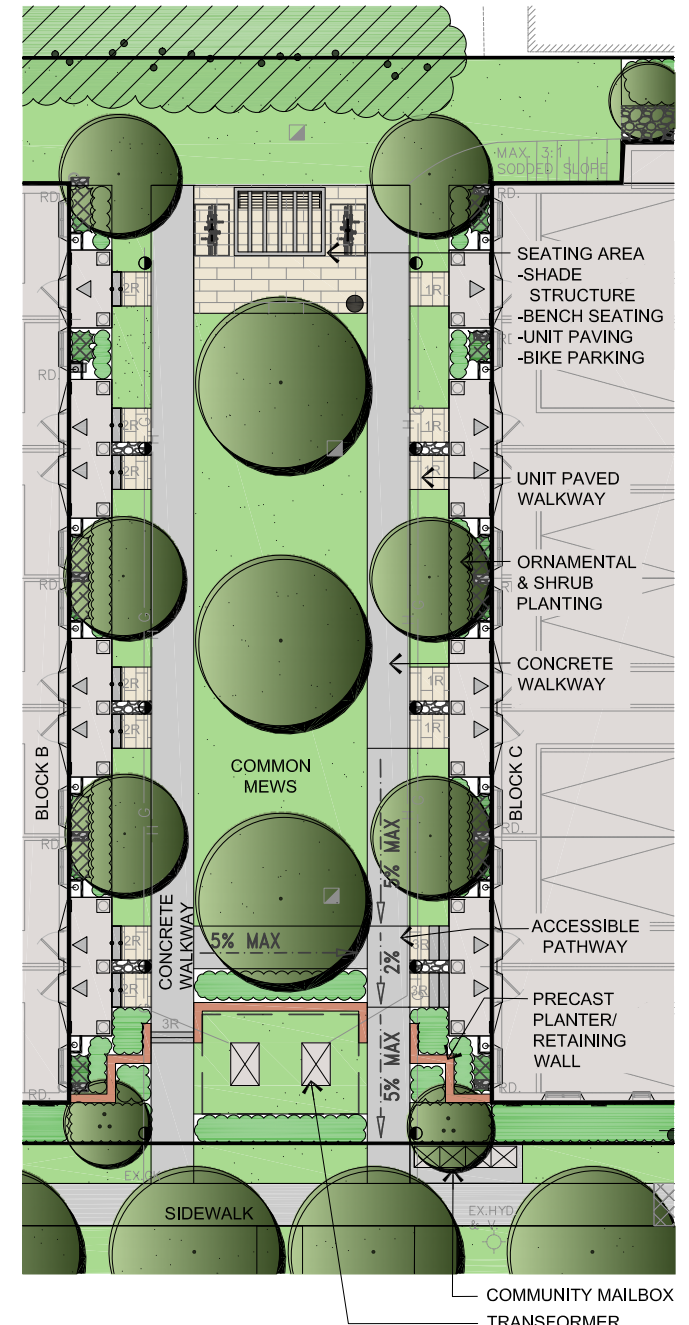


Figure 18: Outdoor Mews Between Blocks

2.4 Parking

- 48 parking spaces (1 per unit) will be provided for residents in shared at-grade garages accessible from Garden Drive. Optional car stackers will be available for some units.
- Vehicular access from Garden Drive shall be clearly defined.
- 11 visitor and commercial parking spots are proposed in the outdoor surface parking lot at the rear of the mixed-use building Block D.
- Vehicular unit pavers will provide an enhanced paving treatment for the surface level parking lot that will complement the surrounding built form.
- Permeable pavers will be installed in visitor parking spaces to assist in storm water management.

2.5 Utilities

- All utility setbacks shall be in conformance with utility requirements.
- Transformers and HVAC equipment shall be located away from public views and outdoor amenity areas or appropriately screened where feasible.
- Utility meters should be architecturally screened or recessed into a niche for the individual dwelling unit such that they are not highly visible within the public realm. Placement of meters shall comply with local utility company requirements.
- The location of surface utilities will be detailed on the Engineering drawings and Landscape plans during the detailed design phase for Site Plan Approval.

3. BUILT FORM GUIDELINES

3.1 Built Form Principles

Height and Massing

The built form consists of a total of 4 townhouse blocks on a corner lot where 3 blocks are a typical back to back townhouse configuration and the fourth being mixed-use residential and commercial facing Lakeshore Road West.

Each of the 3 typical townhouse blocks (Block A, B and C) contain 14 townhomes and share a common at grade garage entry that faces Garden Drive. Each townhome has individual main entrances facing the interior of the site. The mixed-use block (Block D) contains 6 townhomes with individual garages and entries at the rear within the site, while the 2 commercial units face the main street for pedestrian access from main public sidewalks.

All townhouse blocks are four storeys with roof top terrace access. The first storey for the 3 typical townhouse blocks is the common element garage and townhome entrances, with 3 additional storeys above for the main living spaces. There are roof terraces providing private outdoor space for each unit. Block D has one storey commercial units along Lakeshore Road West and at the rear are the individual entries for the townhomes. Above the commercial units are three storeys of residential living space and a roof top terrace. The townhouse portion above the commercial units are stepped back further from the commercial units along the Lakeshore Road West frontage.



Figure 19: Proposed Buildings from Garden Drive

At the fourth storey for all townhouse blocks, the roof line is low so that the interior spaces are within the roof with natural light being provided by dormers. The dormers are a mix of windows or recessed balconies with glass doors. The roof line extends up to the roof terraces allow them to also be less visible from street view below.

Setbacks and Building to Street Ratio

The orientation of the blocks are perpendicular to Garden Drive with minimal street flanking setbacks. This allows for increased interior setbacks and a minimum of 15m setback between the townhouse blocks. There is a greater setback between block C and Block D to allow for exterior at grade commercial/visitor parking.

Transition to Adjacent Land Uses and Community Context

Having a mixed-use commercial and residential block situated at the corner allows for the transition from commercial spaces facing Lakeshore Road West to the residential townhouses blocks facing Garden Drive.



Figure 20: Proposed Building C from the driveway located off Garden Drive

Streetwall and building treatment at grade (the pedestrian experience)

The commercial units facing Lakeshore will have a first storey of 4.5m high and will have larger picture windows and entrances with covered canopies to animate the main street.

All townhouse units have grade related covered portico entries. Most unit entries are paired to allow greater opportunities for landscaping between.

Facade treatments, architectural elements and materials

These townhouse blocks are designed in a transitional Georgian style, with classic detailing consisting of masonry brick, precast and Eifs, accented with dark windows and dark grey roof. The proposed materials are complementary to the area and provide interest and variation to the facades by creating visual breaks.

There is a mix of brick colours between the blocks. Two will use a red brick mix and alternating blocks will have a light coloured tan brick to provide distinctions between the different blocks. At the fourth storey, the roof line is low so that the interior spaces are within the roof with natural light being provided by dormers.

Corner and/or landmark/gateway building treatment

The corner of the mixed used commercial and residential block respects the daylight road widening by having a 1 storey round corner window feature at the commercial unit. With the townhouses above stepped back from the Lakeshore Road West frontage this corner had the opportunity for a unique feature to the development.



Figure 21: Proposed Building D from the intersection of Garden Drive and Lakeshore Road West



Figure 22: Proposed Building D from Lakeshore Road West



Figure 23: Proposed Private Mews & Building B from Garden Drive

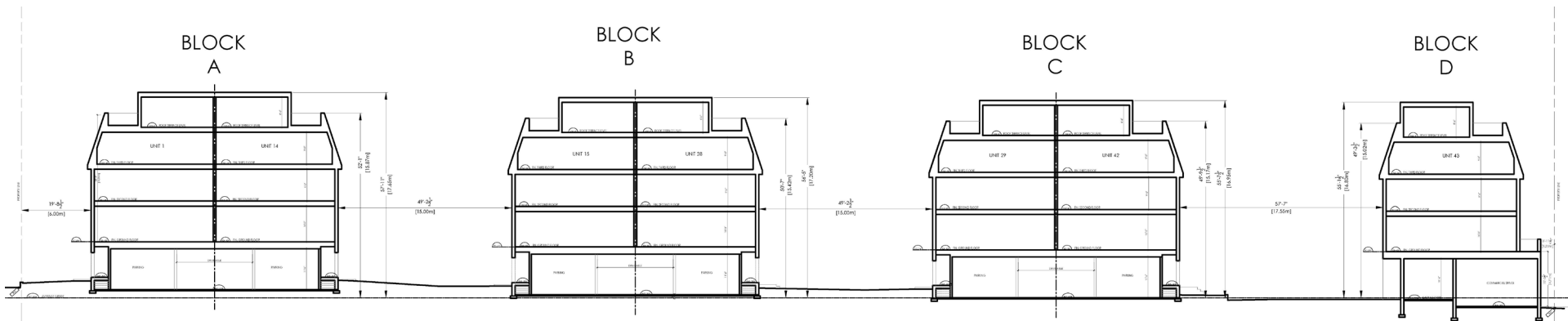


Figure 24: Site Section - North-South

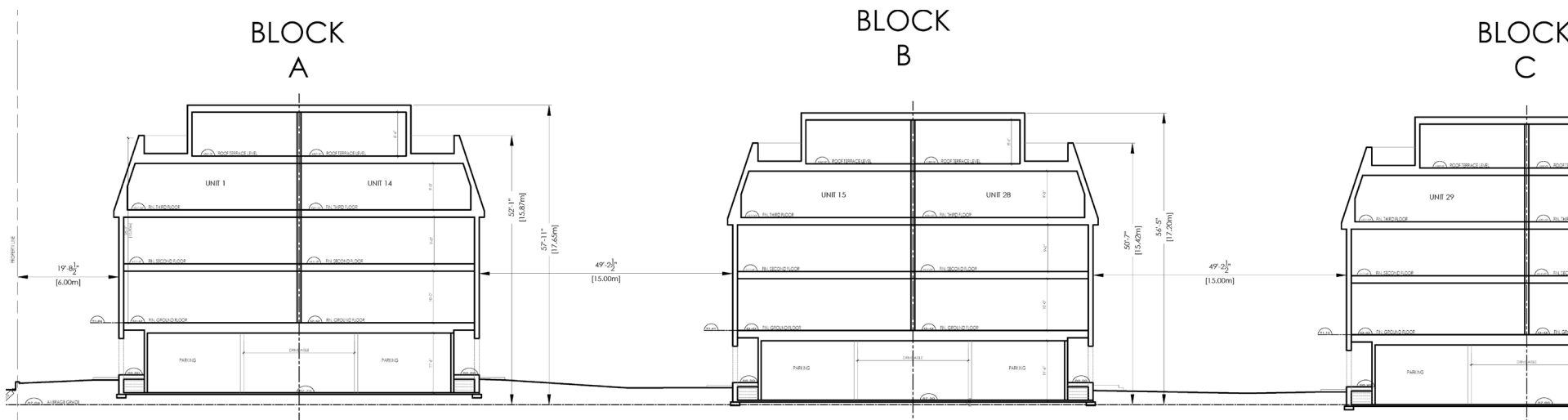


Figure 25: Site Section - Block A, B, part of C

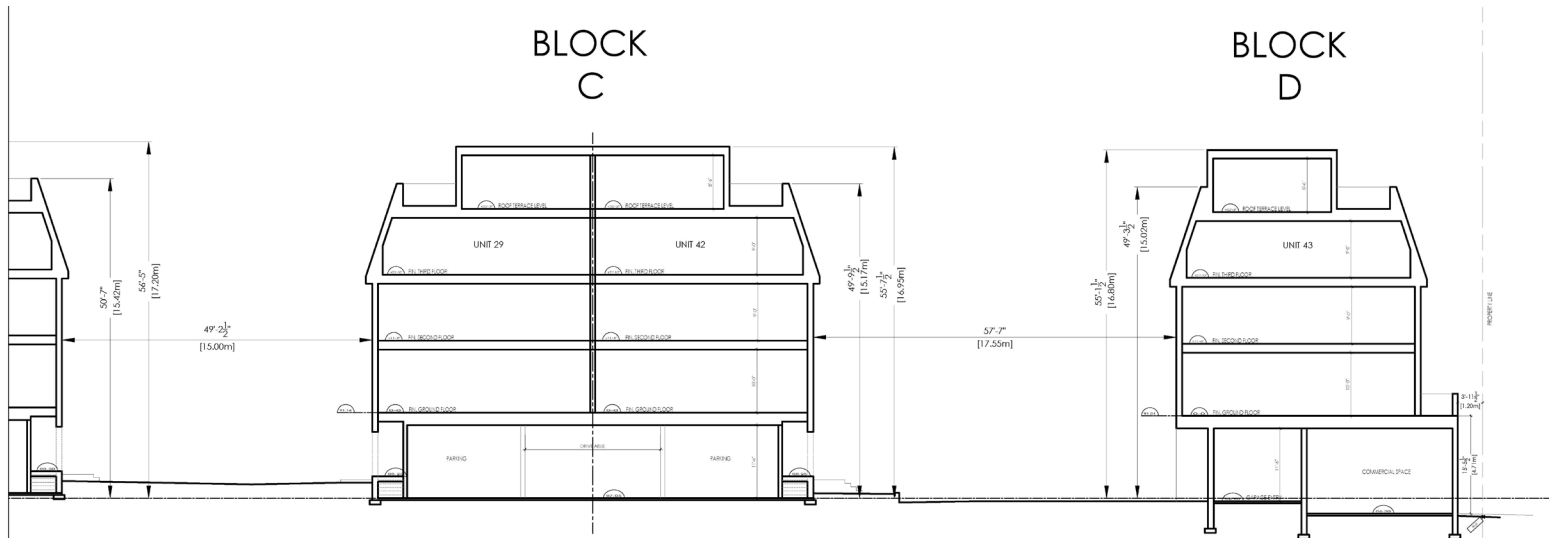


Figure 26: Site Section - Block C, D

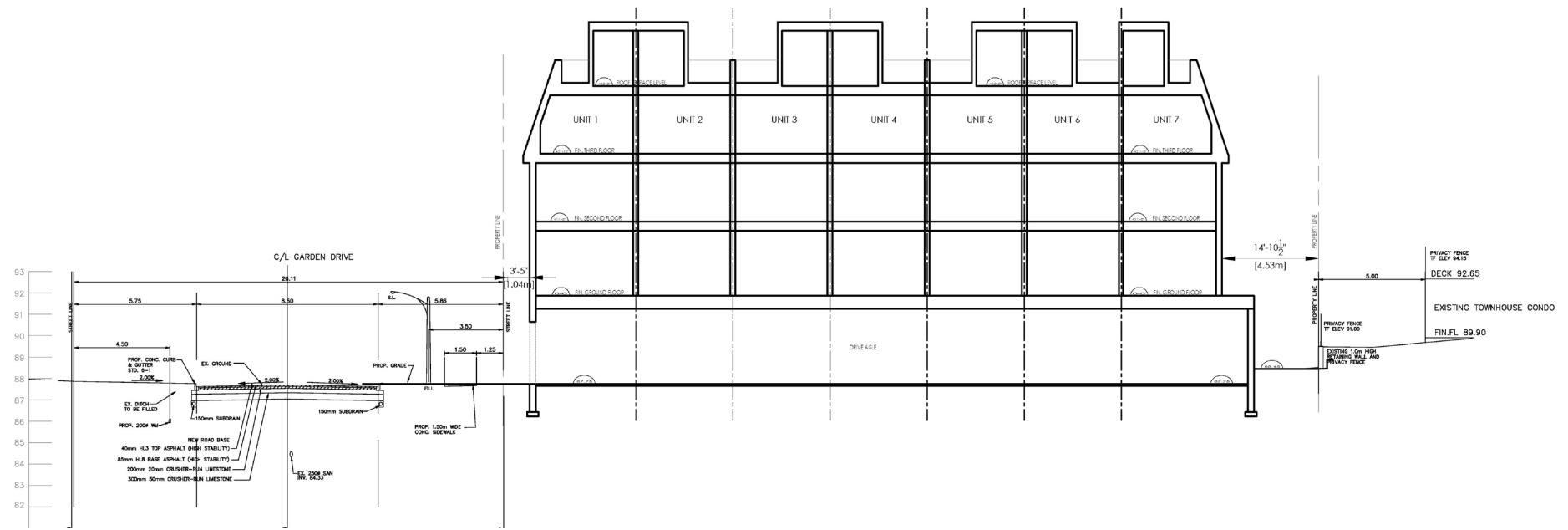


Figure 27: Site Section - Block A - East-West

3.2 Sun/ Shadow Study

The Sun/Shadow Study for the proposed development was prepared by Richard Wengle Architect Inc.

Study images were analyzed for shadow impacts on neighbouring streets, open spaces, natural areas, and other shade sensitive properties. Shadow impacts caused by the proposed development are deemed acceptable as minimal impacts are seen on neighbouring streets, shadow sensitive properties, and open space.

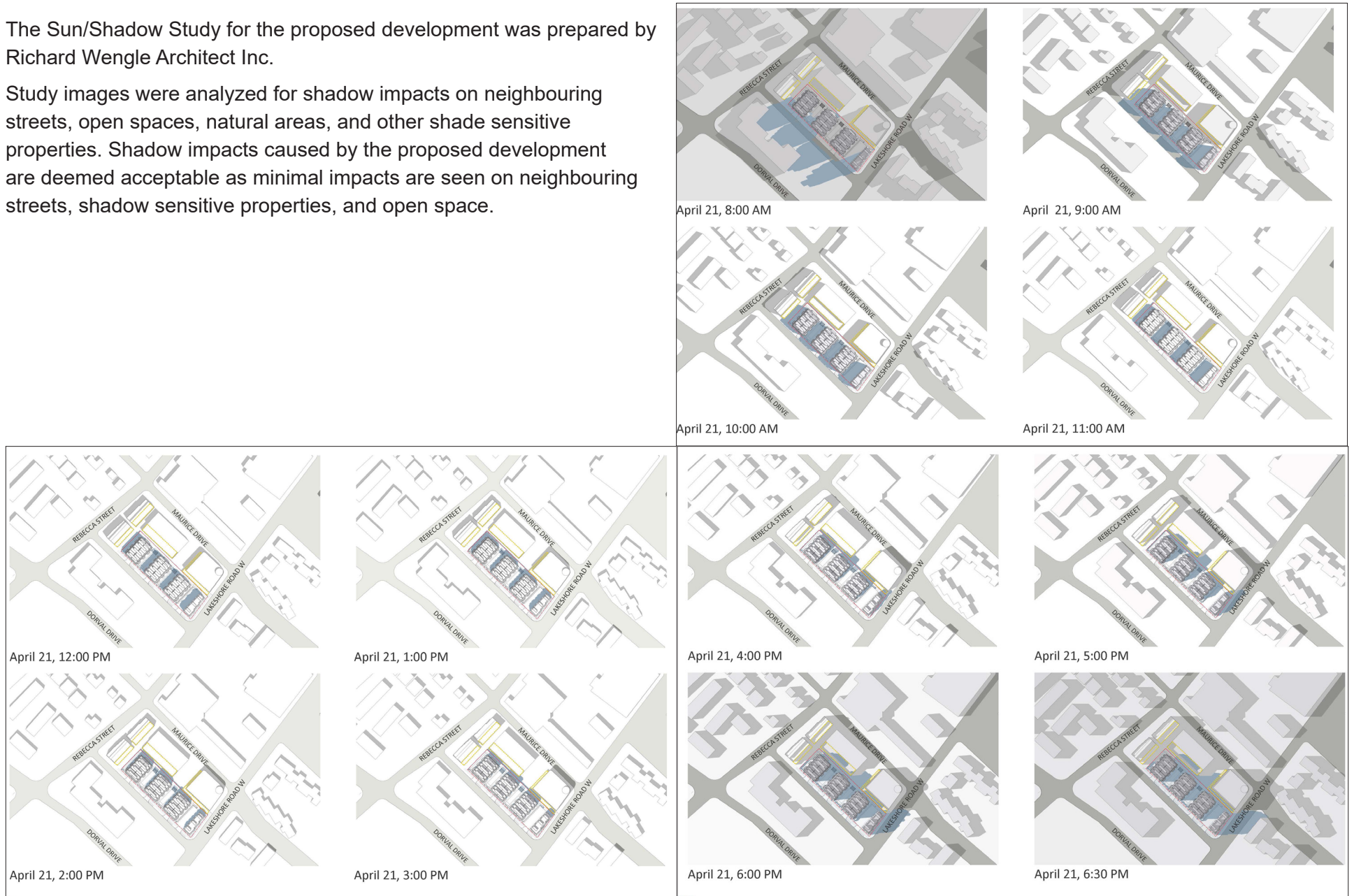


Figure 28: Shadow Analysis During April

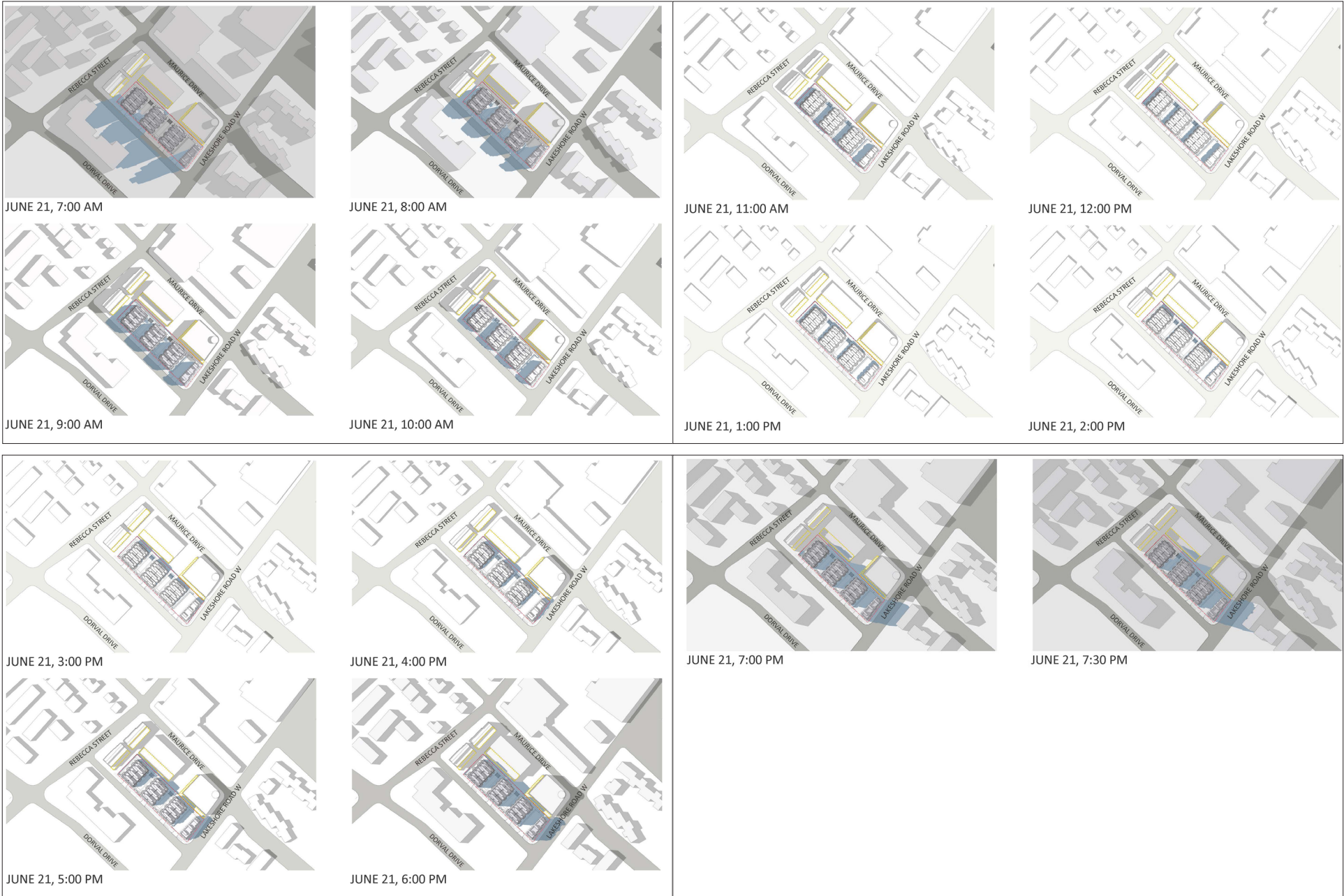


Figure 29: Shadow Analysis During June

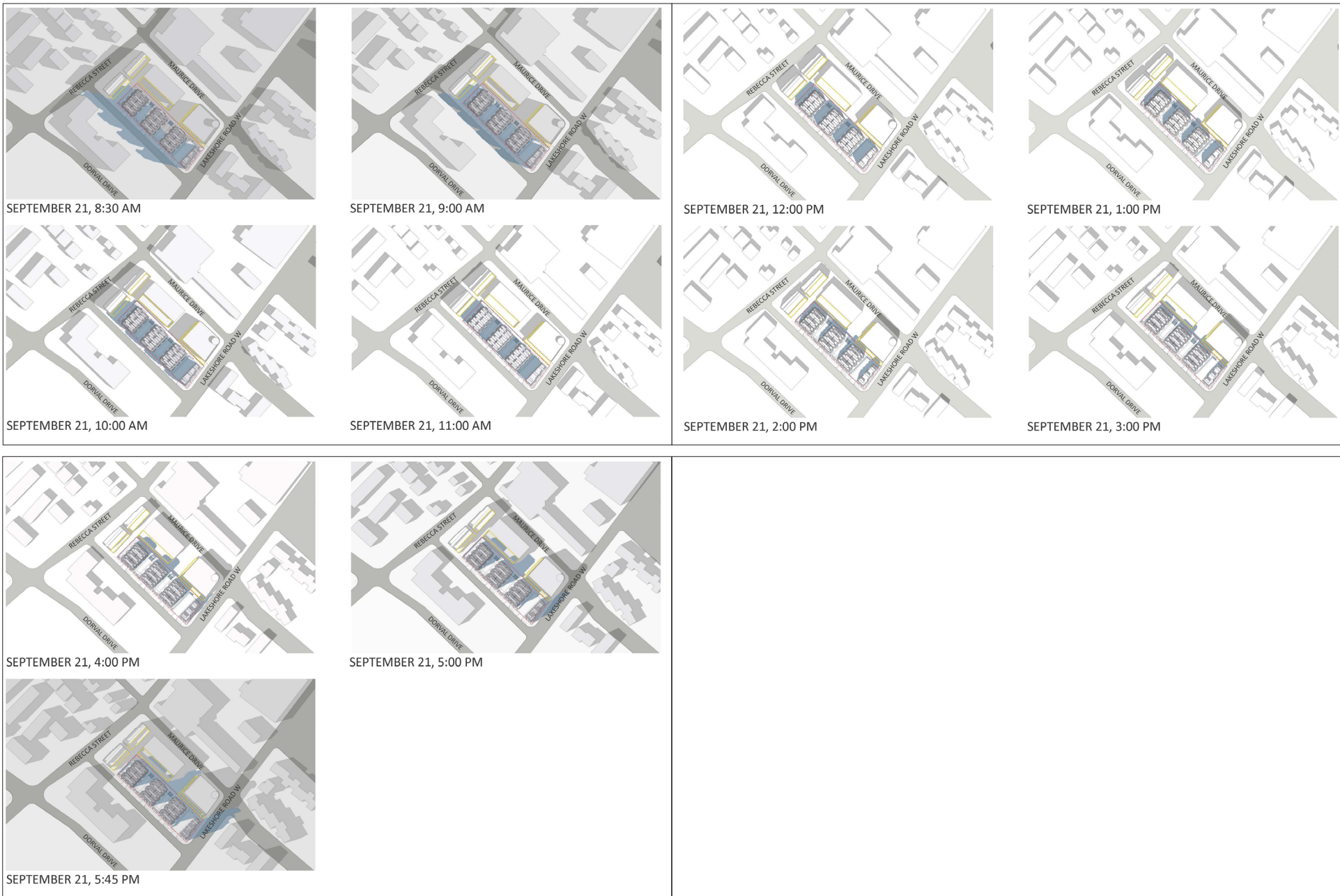


Figure 30: Shadow Analysis During September



Figure 31: Shadow Analysis During December

4. SUSTAINABILITY

4.1 Landscape

The following sustainable practices may be considered:

- Implementation of Low Impact Design Standards for the treatment of stormwater and to reduce stormwater runoff.
- Specification of native drought tolerant, hardy, disease and pest resistant plant species that will contribute to a sustainable and increased urban tree canopy.
- Specification of locally sourced materials and manufactured components where possible.
- Selection of building materials which include sustainable features and/or qualities where possible ie. Recycled materials, etc.
- Provision of high-albedo paving for pedestrian walkways, outdoor amenity areas, and surface parking areas.
- Provide convenient opportunities for alternative and active transportation ie. Provision of bicycle racks and storage.
- Permeable pavers in visitor parking area.
- Enhanced larger tree pits.
- Passive irrigation trenches fed by roof leaders.

4.2 Building

The following sustainable practices may be considered:

- The following sustainable development features may be considered for the building:
- Water efficient fixtures,
- Energy efficient lighting & HVAC systems
- Locally sourced building material (as much as possible).
- Materials with recycled content used as much as possible.

5. IMPLEMENTATION

5.1 Design & Approval Process

This Urban Design Brief provides overall general design direction for the private and public realm components of the proposed mixed-use development. The proposed development shall follow the Town of Oakville's planning and development review processes. Detailed architectural and landscape plans based on the Urban Design Brief will be submitted as part of the Site Plan Approval application to the Town for review and approval.

The property owner shall comply with the approved Urban Design Brief and applicable provisions, principles, and objectives described in the Town of Oakville's Development Design Guidelines. The development design process requires proposals to be consistent with the Town's vision and development design objectives. The proposed development addresses the Town's vision and development design objectives by considering the existing community structure, street network, streetscapes, and overall site planning and built form. Detailed architectural and landscape plans based on the Urban Design Brief will be provided with the Site Plan Approval application to the Town.