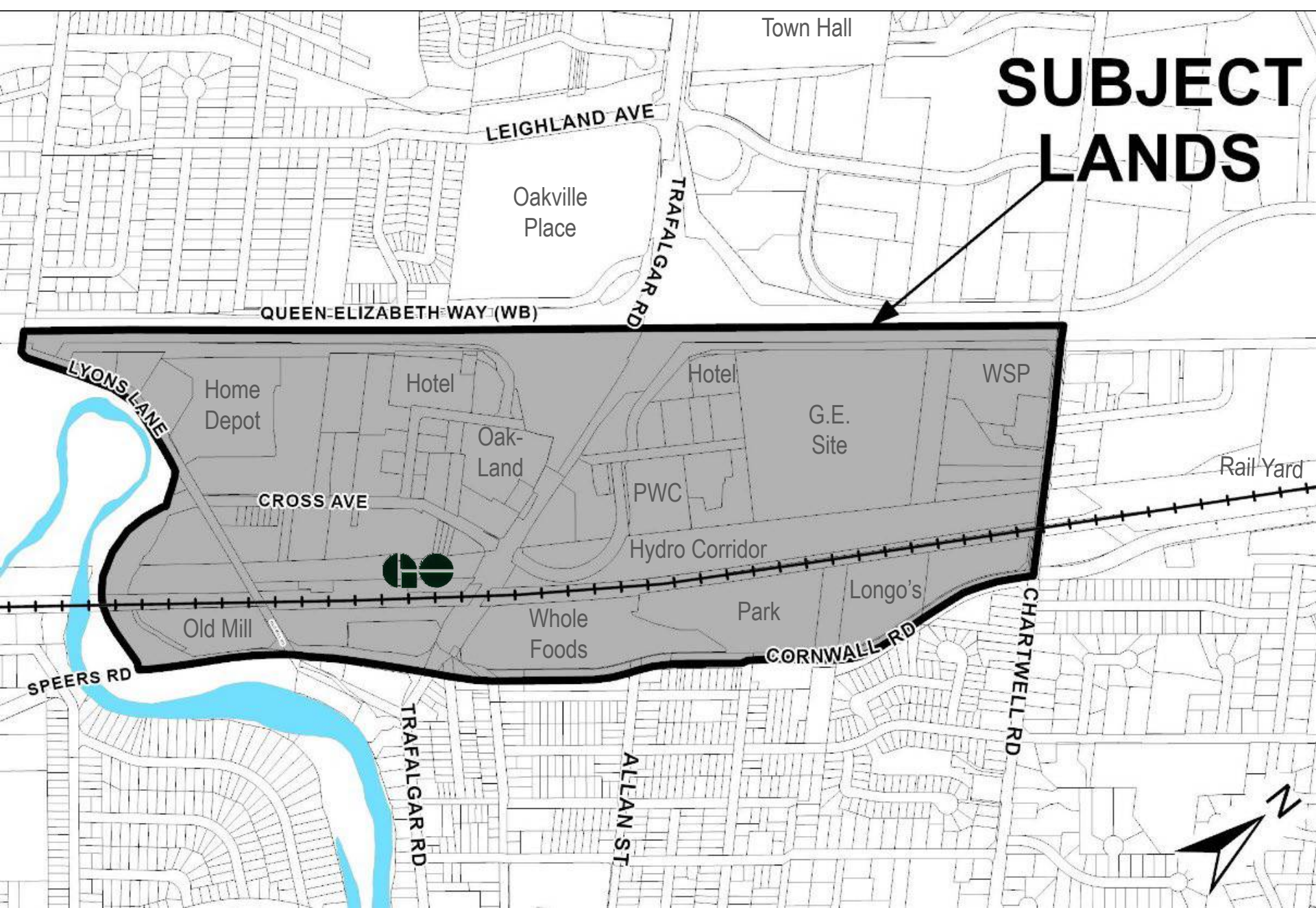




# Midtown Oakville Urban Growth Centre

Town-initiated Official Plan Amendment | File No. 42.15.59  
June 7, 2022 – Statutory Public Meeting

# SUBJECT LANDS



# Purpose

- Present our current thinking
- Continue the discussion
- Listen to what Council, stakeholders and citizens have to say
- Answer questions of clarification

***Council is not making a decision about this OPA tonight.***



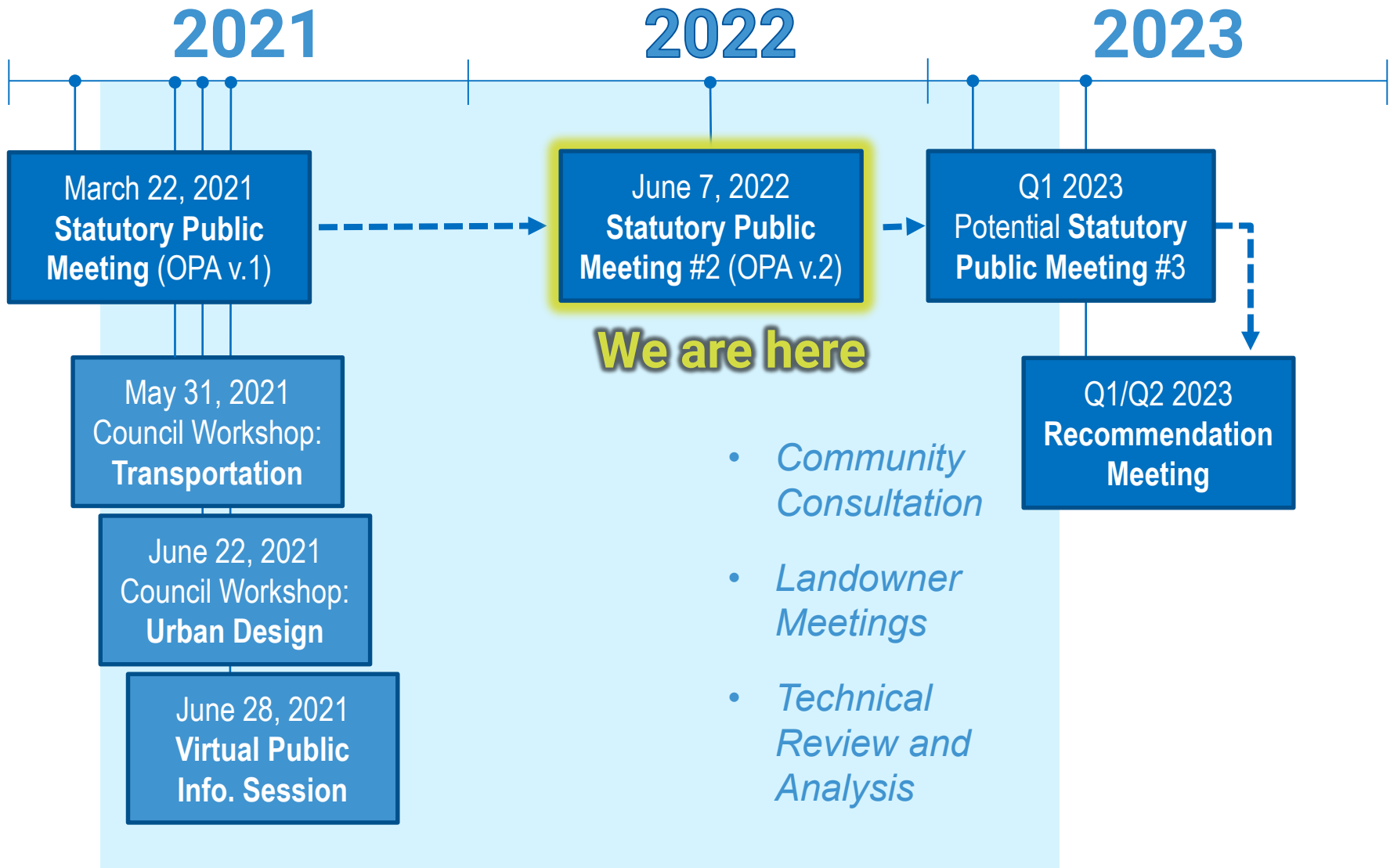
# Vision

Midtown Oakville is being planned to be an **urban community** where people are able to live, work, and play in walkable, mixed use neighbourhoods, connected to the rest of Oakville by pedestrian, cycling, transit and street networks.

It is to be a self-sufficient urban community with tall buildings, open spaces, recreational and retail amenities.



# Midtown OPA Timeline



# Land Use Planning Context

## Province of Ontario

- Planning Act
- Provincial Policy Statement
- Growth Plan, Greenbelt Plan, etc.

## Region of Halton

- Regional Official Plan\*

## Town of Oakville

- Official Plan\*
- Zoning By-law

*\* Currently Under Review*

## Planning Act

- provides that upper-tier municipalities may identify the area around a higher order transit station as a **protected major transit station area**
- **no** longer permits **bonusing** for height and density

## Growth Plan

- requires that Midtown Oakville – a strategic growth area, **urban growth centre** and **major transit station area** – be planned as a complete community
- requires that Midtown Oakville be planned to achieve a **minimum of 200 residents and jobs combined per hectare** by 2031



# Region of Halton

## Regional Official Plan

- **delineates** Halton's **urban growth centres** (UGCs) and **major transit station areas** (MTSAs), including Midtown Oakville
- **requires local official plans** to identify UGCs and MTSAs in accordance with the boundaries delineated by the Regional Official Plan
- **directs** higher-density mixed-use development to these **strategic growth areas**



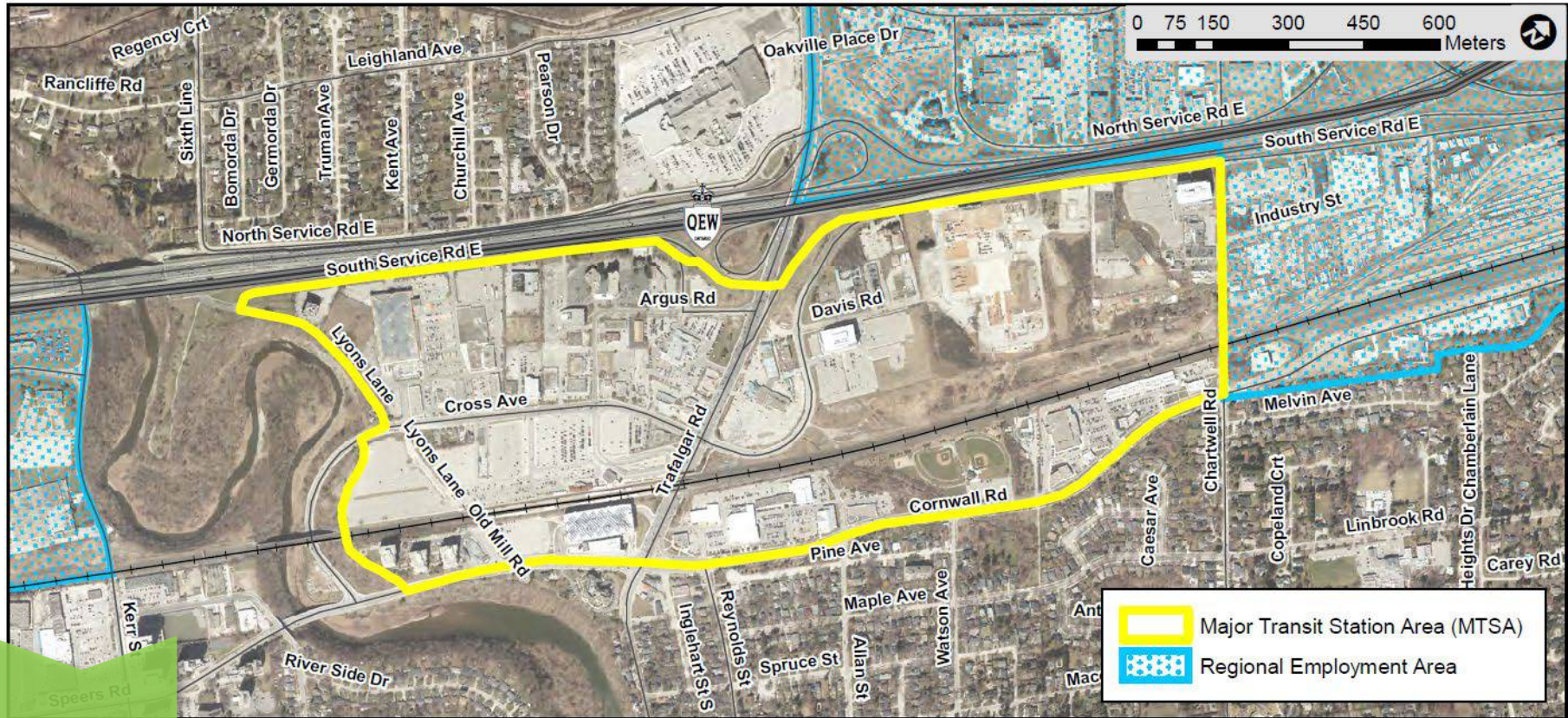


# Region of Halton

## Regional Official Plan

Map 6a - Midtown Oakville GO UGC/MTSA

Approved 2021-11-10



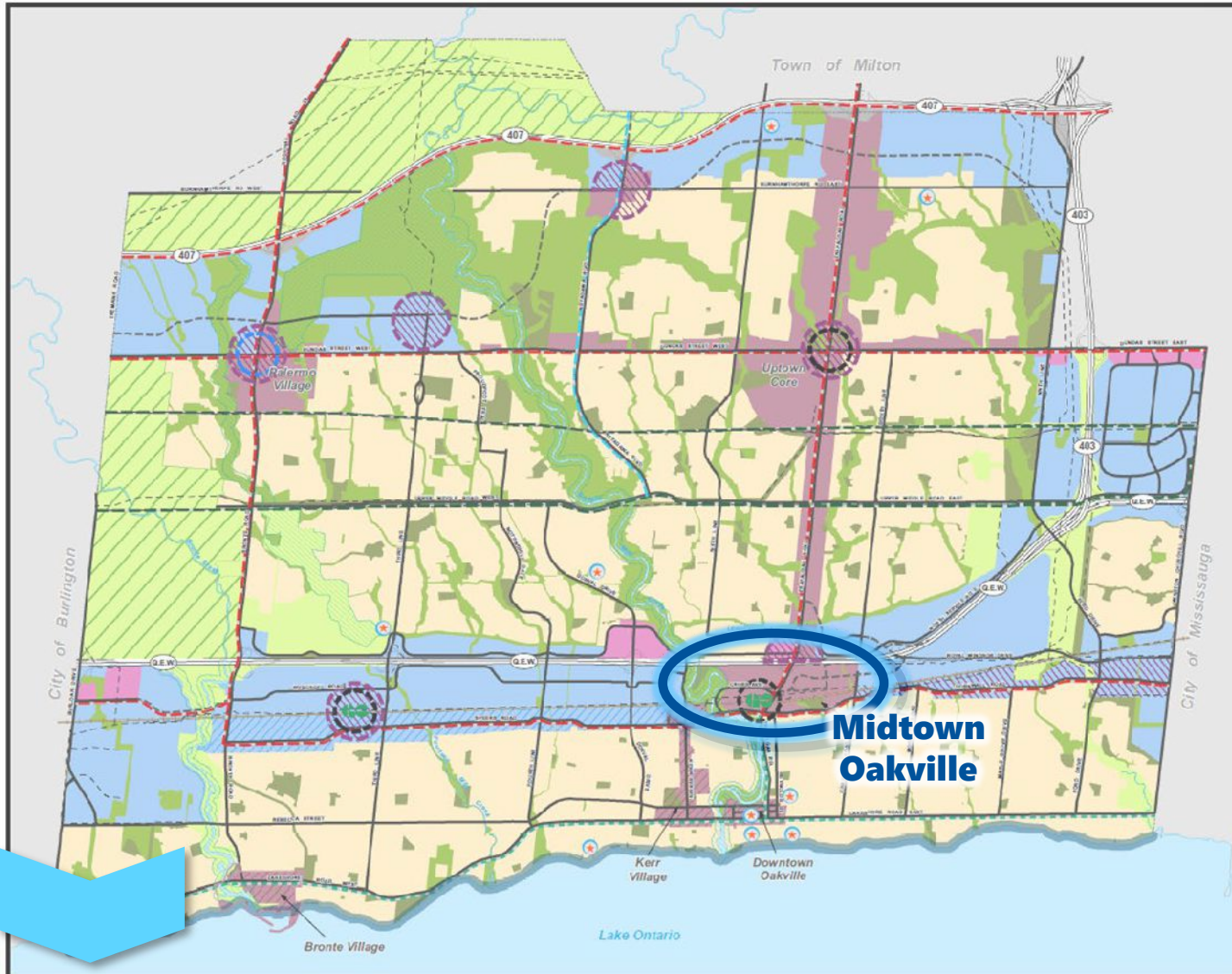
# Region of Halton

## Regional Official Plan

- Require the local municipality to:
  - Include official plan policies to **meet intensification and mixed-use objectives.**
  - Prescribe **minimum development densities.**
- Require policies for strategic growth areas to include:
  - a **transportation network** that integrates active transportation and higher order transit
  - **urban design guidelines** to promote active transportation and transit-supportive uses



# Town of Oakville



### SCHEDULE A1 URBAN STRUCTURE

LEGEND<sup>1</sup>

- PARKWAY BELT
- GREENBELT
- GREENBELT - URBAN RIVER VALLEY
- NATURAL HERITAGE SYSTEM
- PARKS, OPEN SPACE & CEMETERIES
- WATERFRONT OPEN SPACE
- NODES AND CORRIDORS
- EMPLOYMENT AREAS
- MAJOR COMMERCIAL AREAS
- RESIDENTIAL AREAS
- MAIN STREET AREA
- EMPLOYMENT MIXED USE CORRIDOR
- REGIONAL TRANSIT NODE
- PROPOSED REGIONAL TRANSIT NODE
- NODES AND CORRIDORS<sup>2</sup> FOR FURTHER STUDY
- MAJOR TRANSPORTATION CORRIDOR<sup>3</sup>
- PROPOSED MAJOR TRANSPORTATION CORRIDOR
- REGIONAL TRANSIT PRIORITY CORRIDOR
- MOBILITY LINK
- MAJOR ACTIVE TRANSPORTATION CONNECTIONS
- SCENIC CORRIDOR
- UTILITY CORRIDOR
- PROVINCIAL PRIORITY TRANSIT CORRIDOR
- MAJOR TRANSIT STATION
- HERITAGE CONSERVATION DISTRICTS/<sup>4</sup> CULTURAL HERITAGE LANDSCAPES

NOTE 1: This Schedule does not represent land use designations

NOTE 2: In addition, the south side of Dundas is recognized as having the potential for intensification subject further study to more precisely delineate the extent of such areas

NOTE 3: Transportation corridors, with the exception of Provincial highways, permit all transportation modes including transit, pedestrian and bicycle facilities

NOTE 4: Heritage Conservation Districts and cultural heritage landscapes are elements of the urban structure. As additional Heritage Conservation Districts and cultural heritage landscapes are protected and registered under the Ontario Heritage Act, they shall be added to Schedule A1, Urban Structure

1:50,000  
April 17, 2018

© DEPARTMENT OF PLANNING & DEVELOPMENT, OAKVILLE, ONTARIO

# Transportation Planning

## Study

- Midtown EA Study (completed)
- Transportation Master Plan (completed; 2023 update)
- Road network updates/EAs



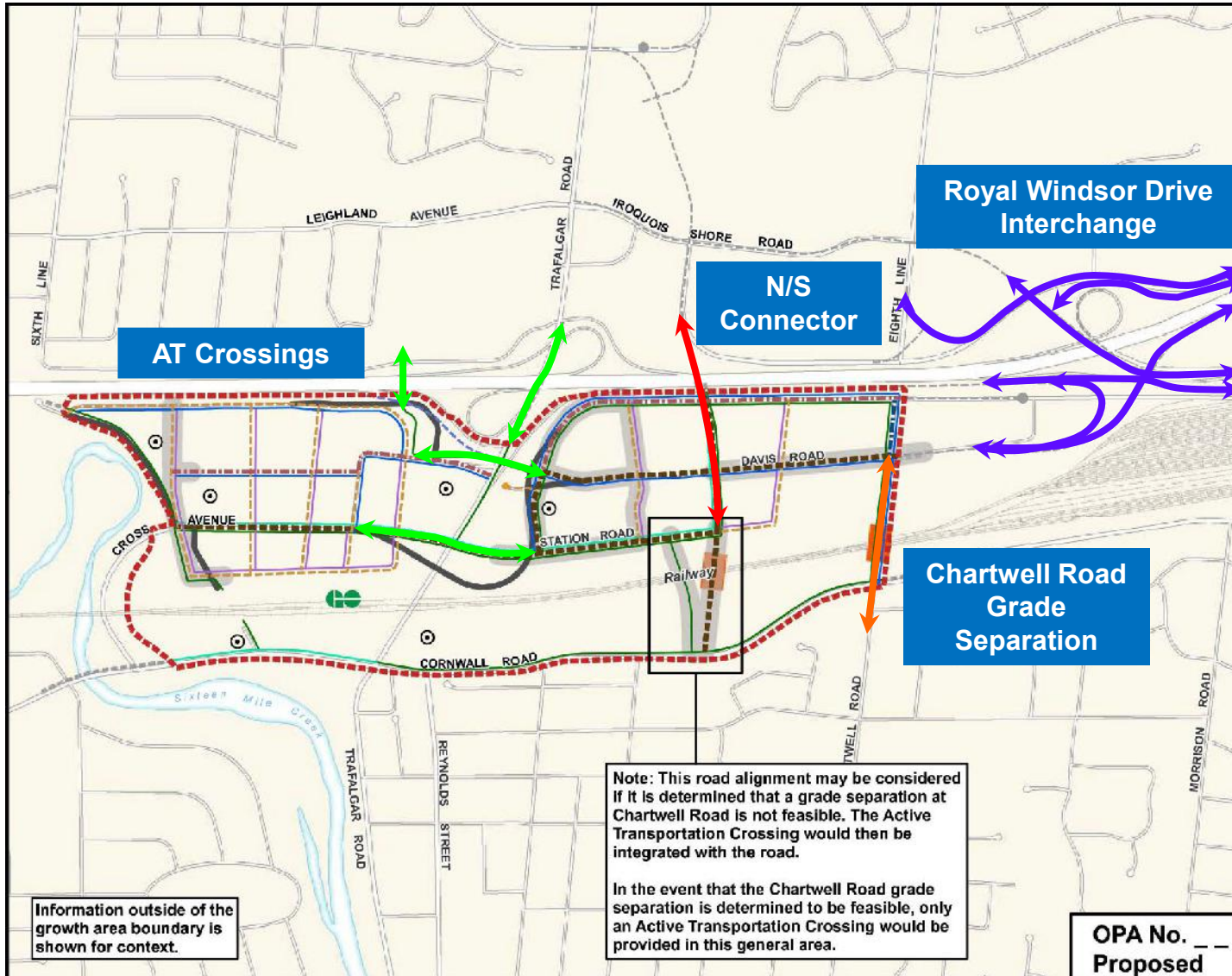
## Design

- North Service Road
- West Active Transportation Crossing
- Royal Windsor Drive Interchange
- Cross Avenue Realignment West
- East Active Transportation Crossing
- Cross Avenue Realignment East
- QEW Off-ramp Realignment

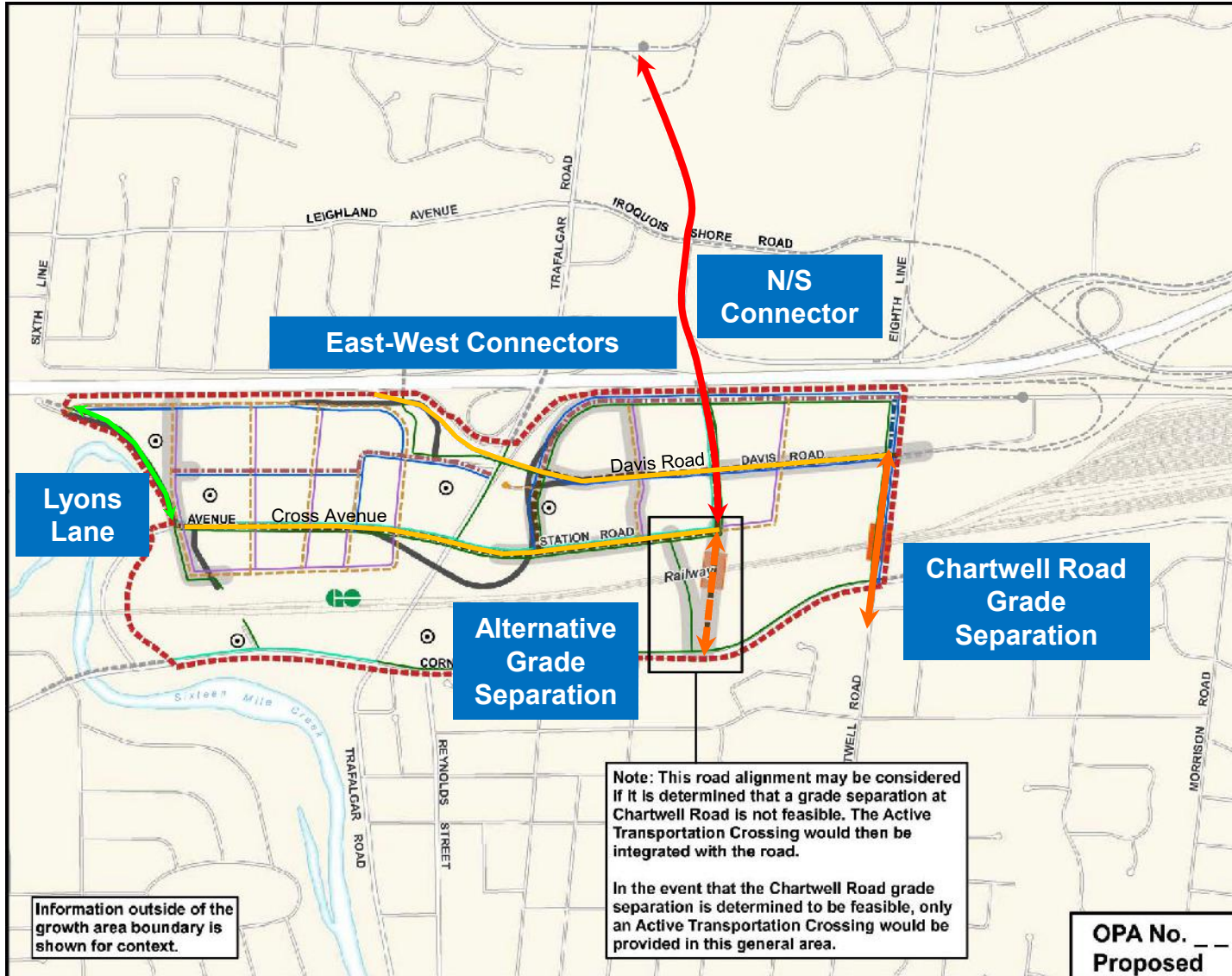


## Implementation

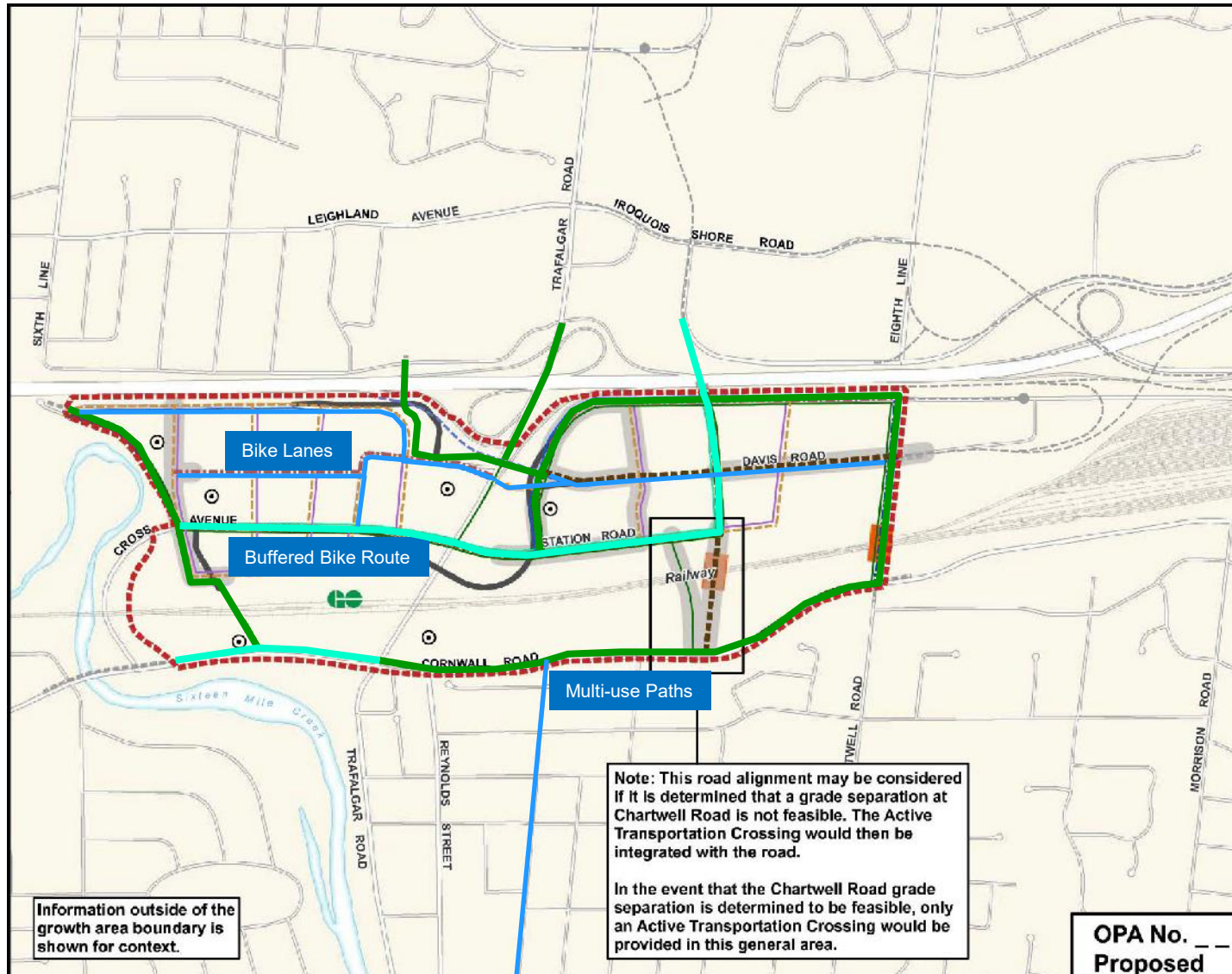
# Transportation: Proposed Schedule L3



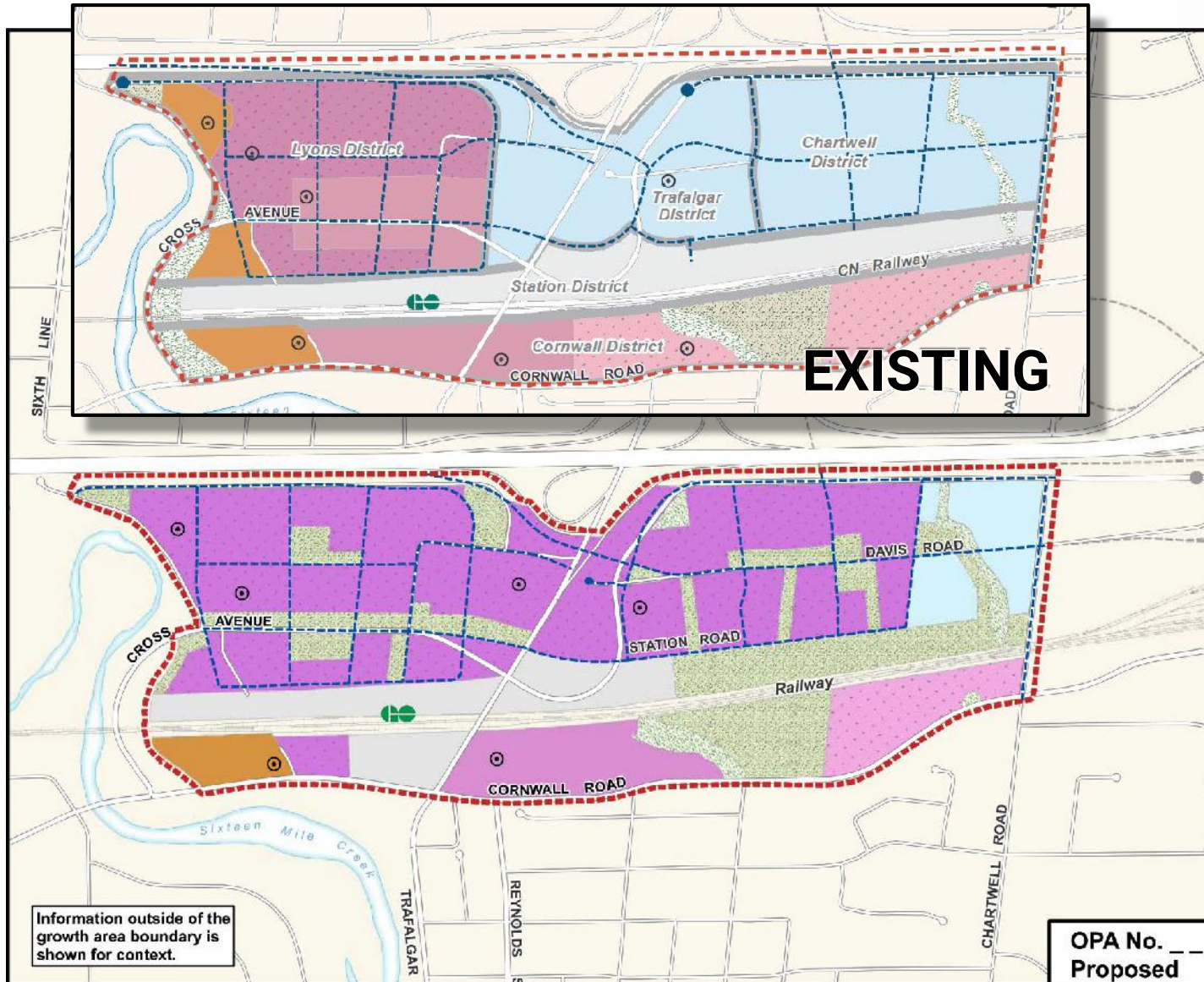
# Transportation: Road Connectors



# Transportation: Active Transportation



# Land Use: Proposed Schedule L1



## SCHEDULE L1 MIDTOWN OAKVILLE LAND USE



- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
  - HIGH DENSITY RESIDENTIAL
  - COMMUNITY COMMERCIAL
  - URBAN CENTRE
  - URBAN CORE
  - OFFICE EMPLOYMENT
  - NATURAL AREA
  - PARKS AND OPEN SPACE
  - UTILITY
  - FUTURE ROADS (OWN TO 50 METERS FROM CURB)
  - FUTURE CUL-DE-SAC
  - RAILWAY
  - MAJOR TRANSIT STATION
- REFER TO PART B, MIDTOWN OAKVILLE, FOR GROWTH AREA POLICIES
- REFER TO PART E, MIDTOWN OAKVILLE, FOR GROWTH AREA POLICIES



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MAY 12, 2022

OPA No. \_\_  
Proposed

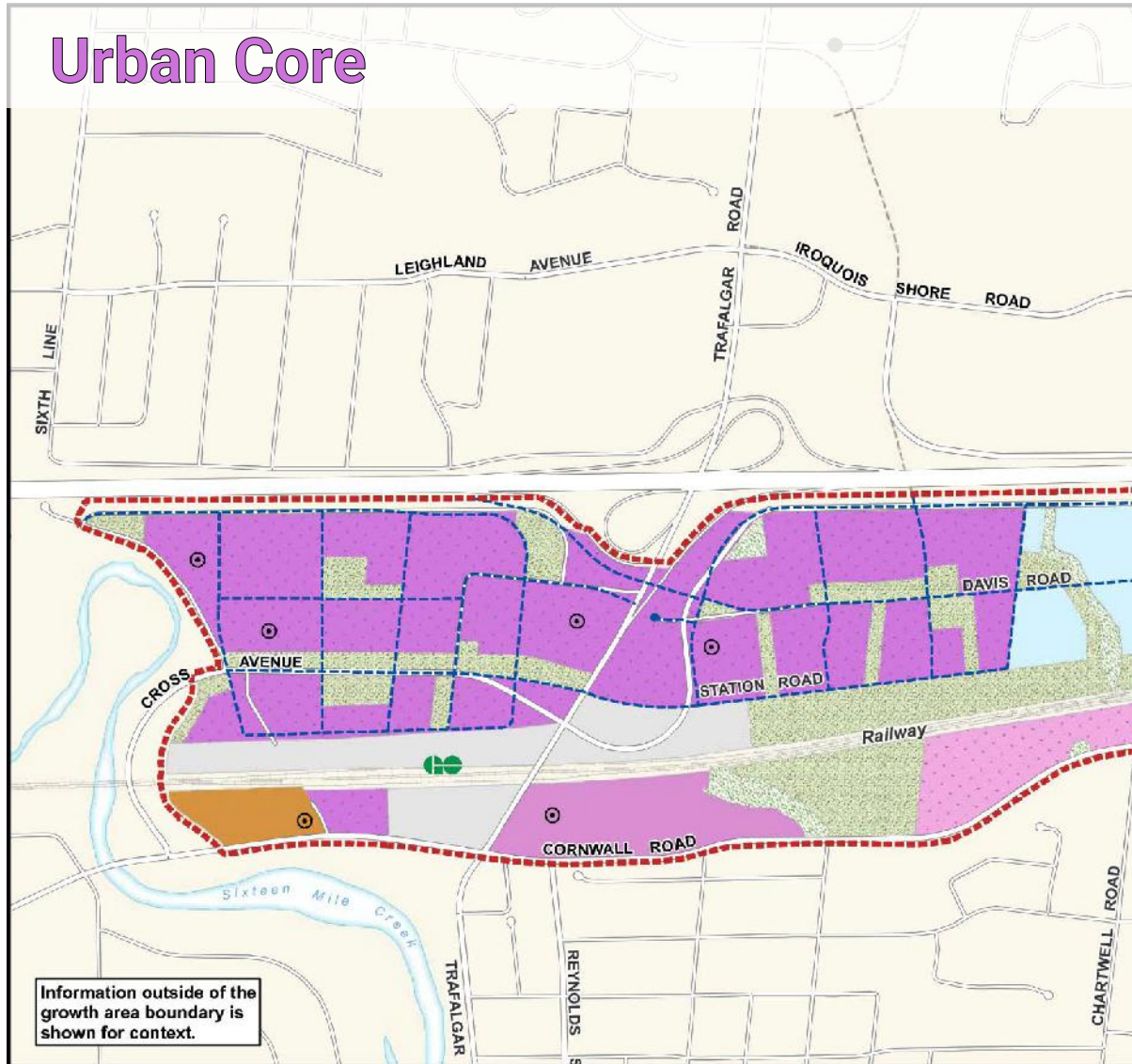
Information outside of the growth area boundary is shown for context.

3. SUPPLEMENTARY PLANNING TOOL, LAND USE, OAKVILLE 2022. L1 - Midtown\_Oakville\_Regional\_Municipal\_Progressive\_Schedule\_L1\_Proposed\_Midtown\_Oakville



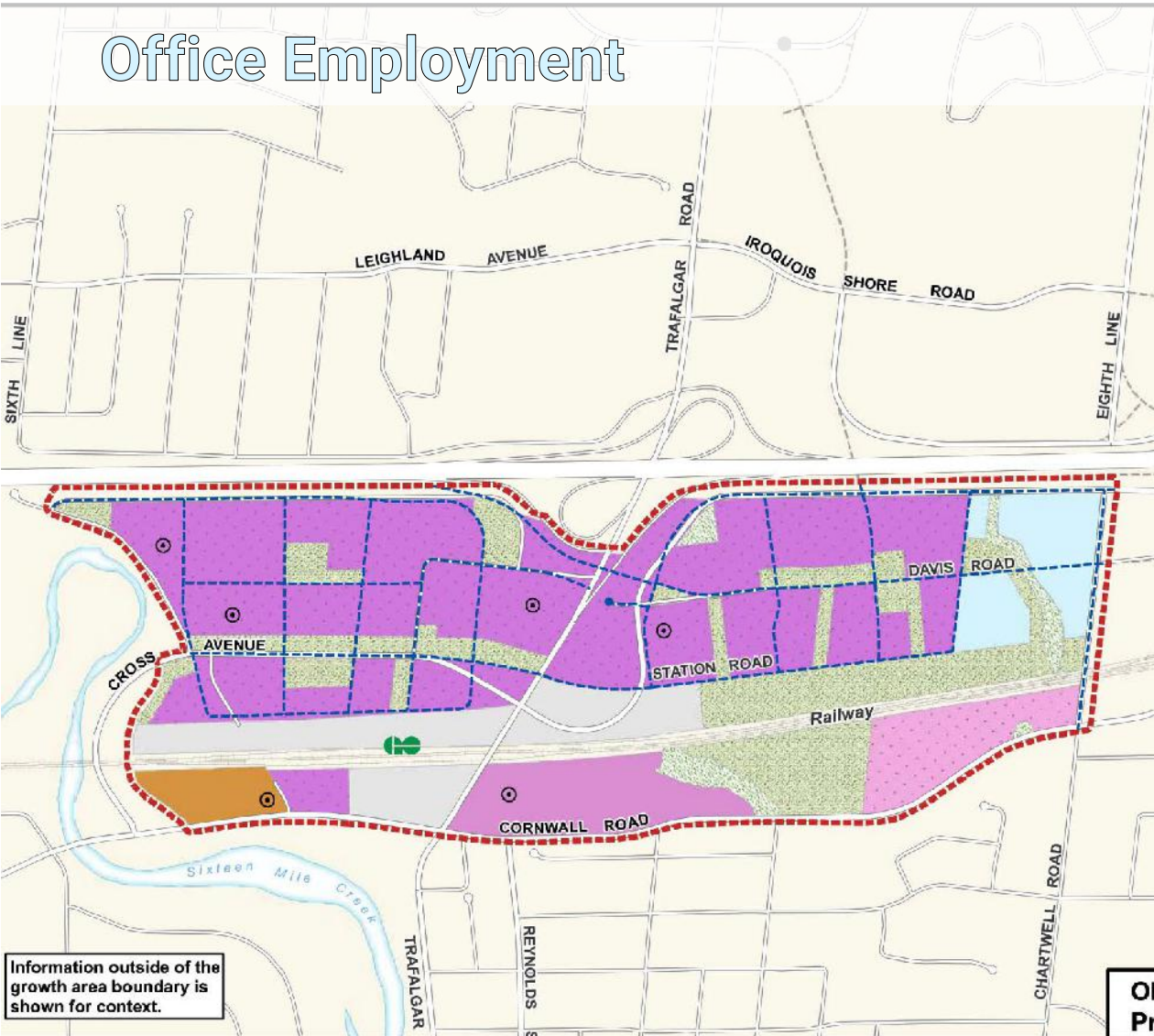
# Land Use: Proposed Schedule L1

## Urban Core

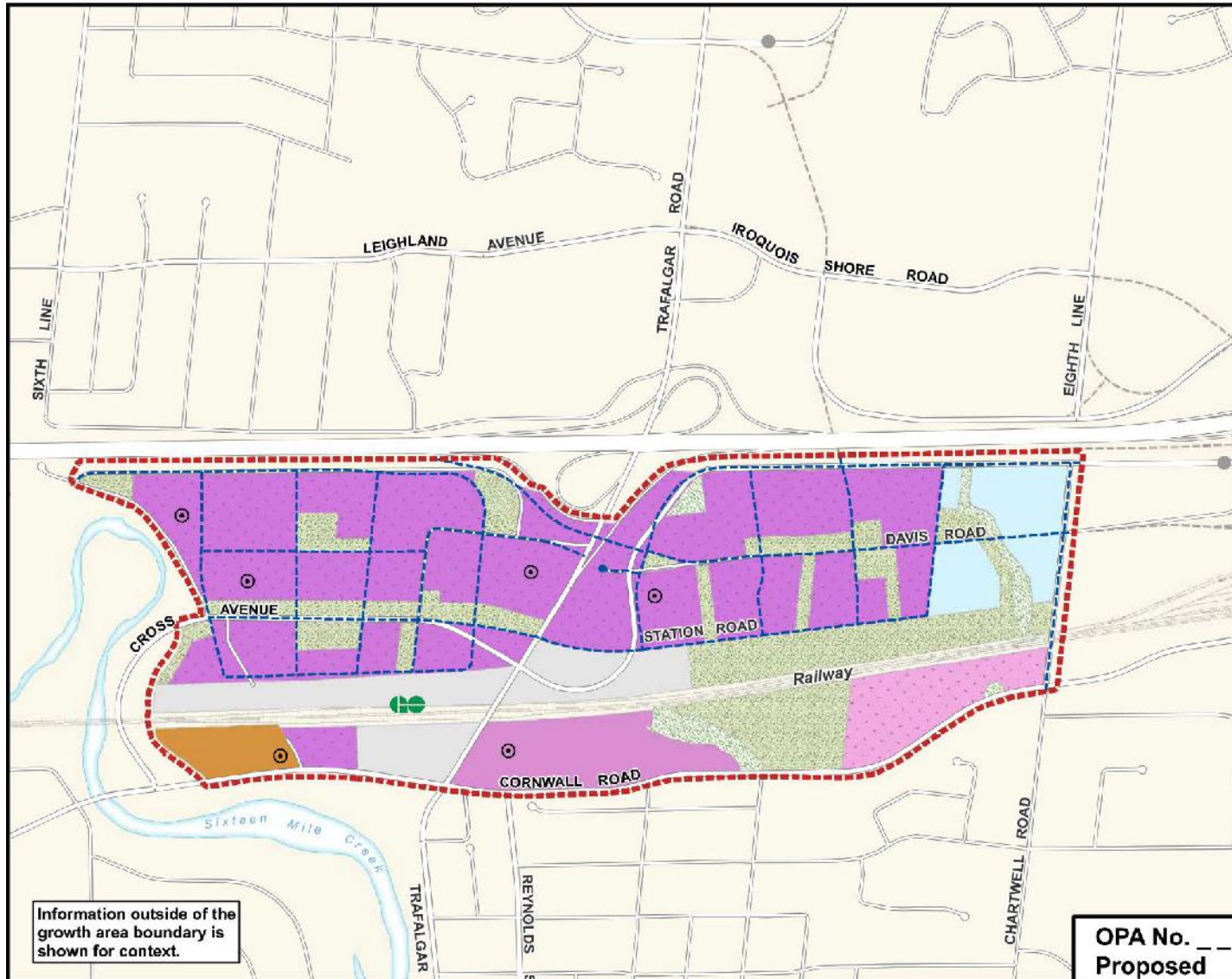


# Land Use: Proposed Schedule L1

## Office Employment



# Land Use: Proposed Schedule L1



### SCHEDULE L1 MIDTOWN OAKVILLE LAND USE

- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
- HIGH DENSITY RESIDENTIAL
- COMMUNITY COMMERCIAL
- URBAN CENTRE
- URBAN CORE
- OFFICE EMPLOYMENT
- NATURAL AREA
- PARKS AND OPEN SPACE
- UTILITY
- FUTURE ROADS (OWN TO 50% COMPLETE)
- FUTURE CUL-DE-SAC
- RAILWAY
- MAJOR TRANSIT STATION

REFER TO PART B, MIDTOWN OAKVILLE, FOR GROWTH AREA POLICIES

⊙ REFER TO PART E, MIDTOWN OAKVILLE, FOR GROWTH AREA EXCEPTIONS

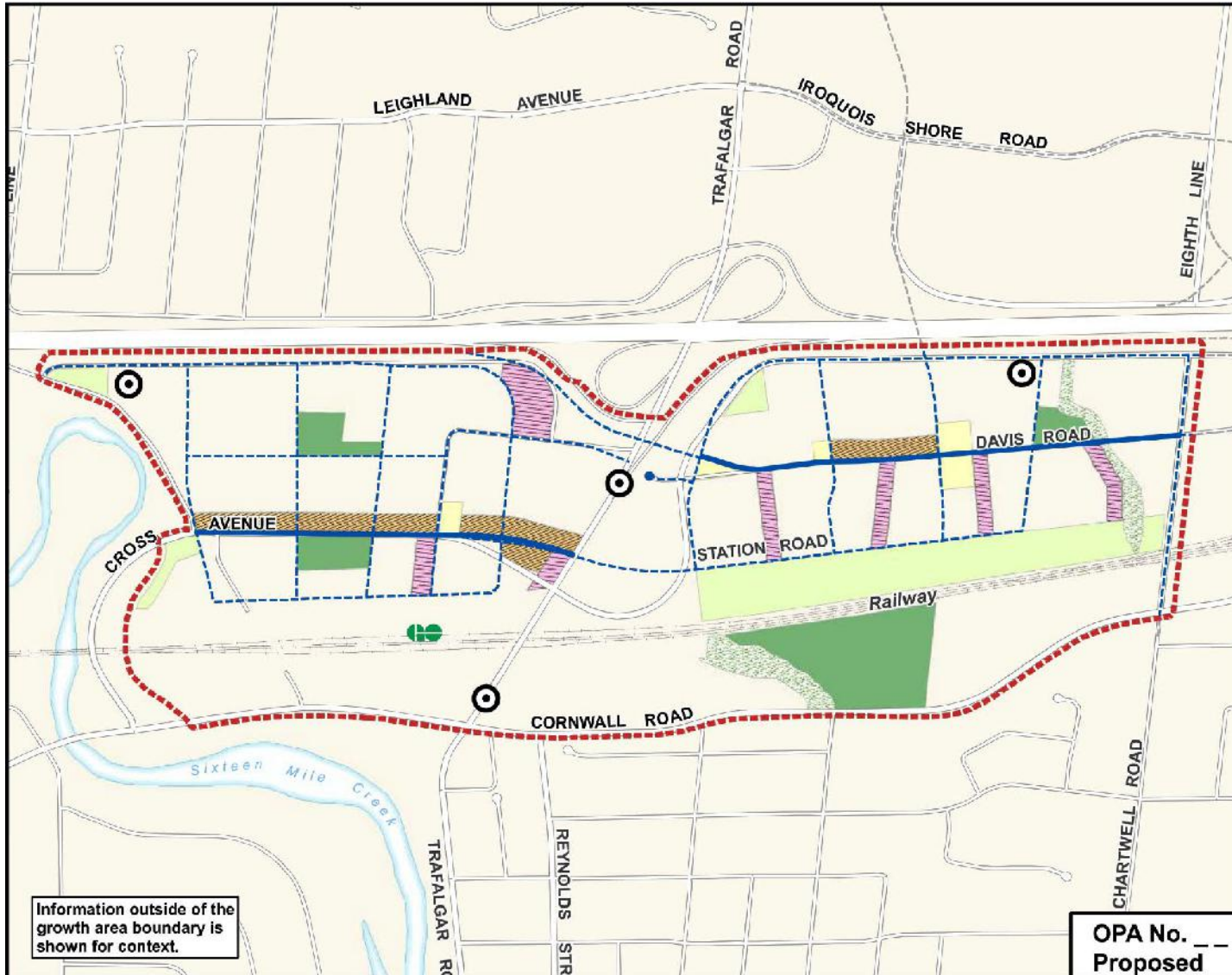
1:7,500  
MAY 12, 2022

OPERA No. --  
Proposed

3. INFORMATION: PLANNING & DEVELOPMENT, CITY OF OAKVILLE, 2022. LAND USE, OPA, REGIONAL MUNICIPALITY OF OAKVILLE, SCHEDULE L1, PROPOSED MIDTOWN OAKVILLE

Information outside of the growth area boundary is shown for context.

# Public Realm: Proposed Schedule L4



Information outside of the growth area boundary is shown for context.

OPA No. --  
Proposed

### SCHEDULE L4 MIDTOWN OAKVILLE PUBLIC REALM

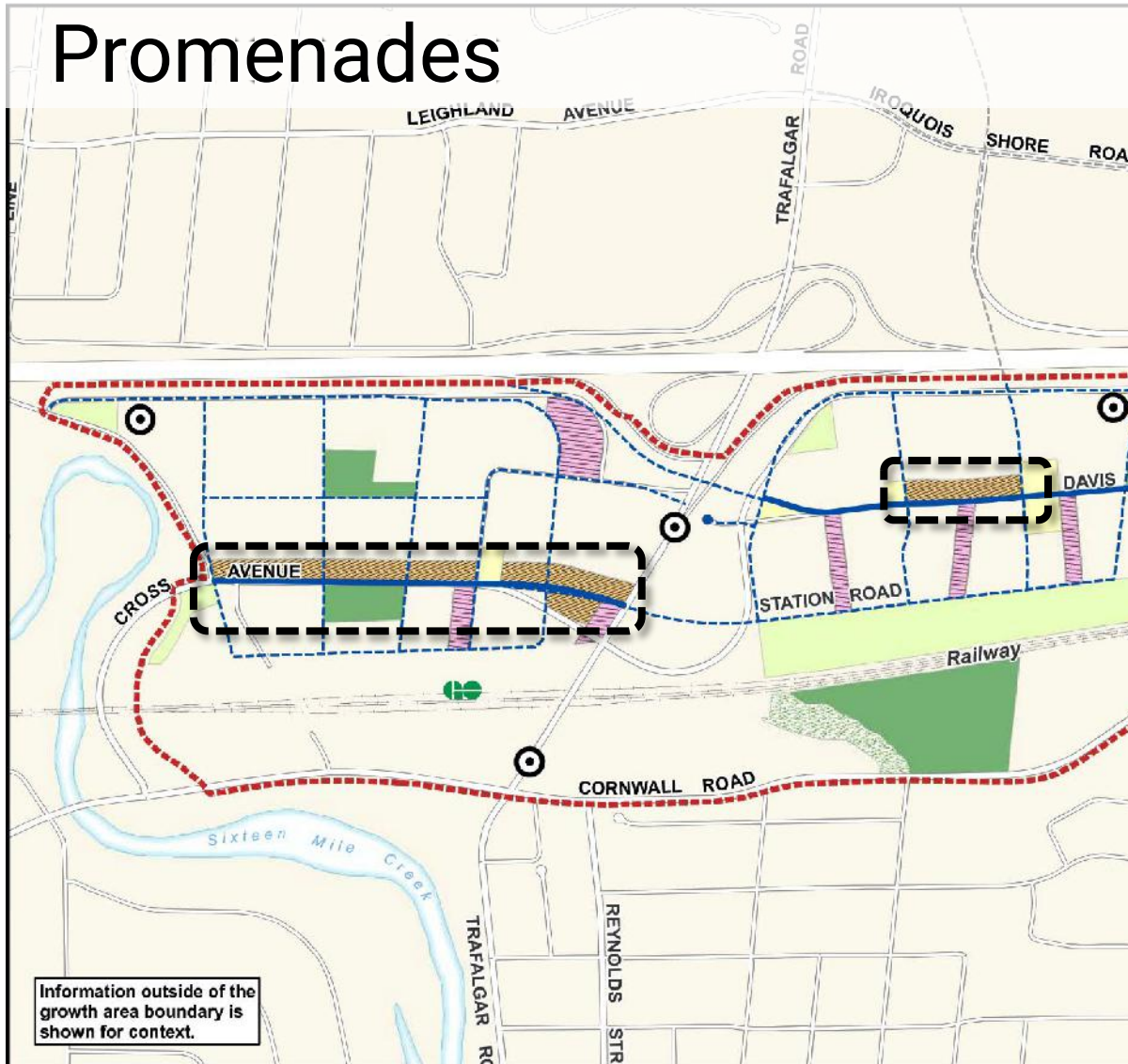
- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
- MAIN STREET
- GATEWAY
- PROMENADE
- URBAN SQUARE
- PUBLIC COMMON
- CONNECTOR
- OPEN SPACE
- NATURAL AREA
- FUTURE ROADS
- RAILWAY
- MAJOR TRANSIT STATION

REFER TO PART B, MIDTOWN OAKVILLE FOR GROWTH AREA POLICIES

1:6,500  
MAY 12, 2022

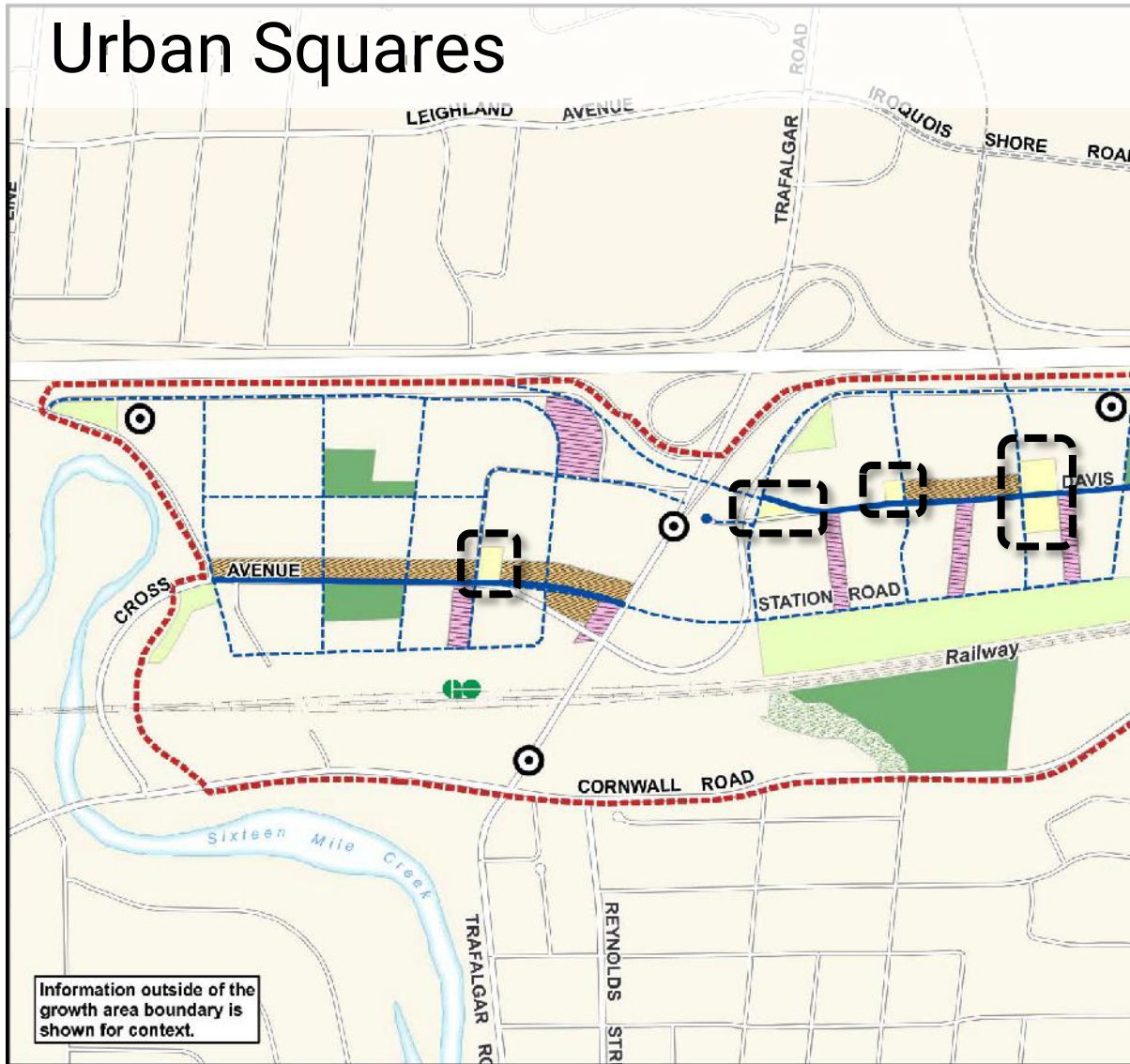
OPACOME PLAN/INTEGRAL/ANALYSIS/CONCEPTUAL/PLANNING/OPA/Realms/Map/OPA/Proposed/Schedule L4/Proposed/Midtown/2022

## Promenades



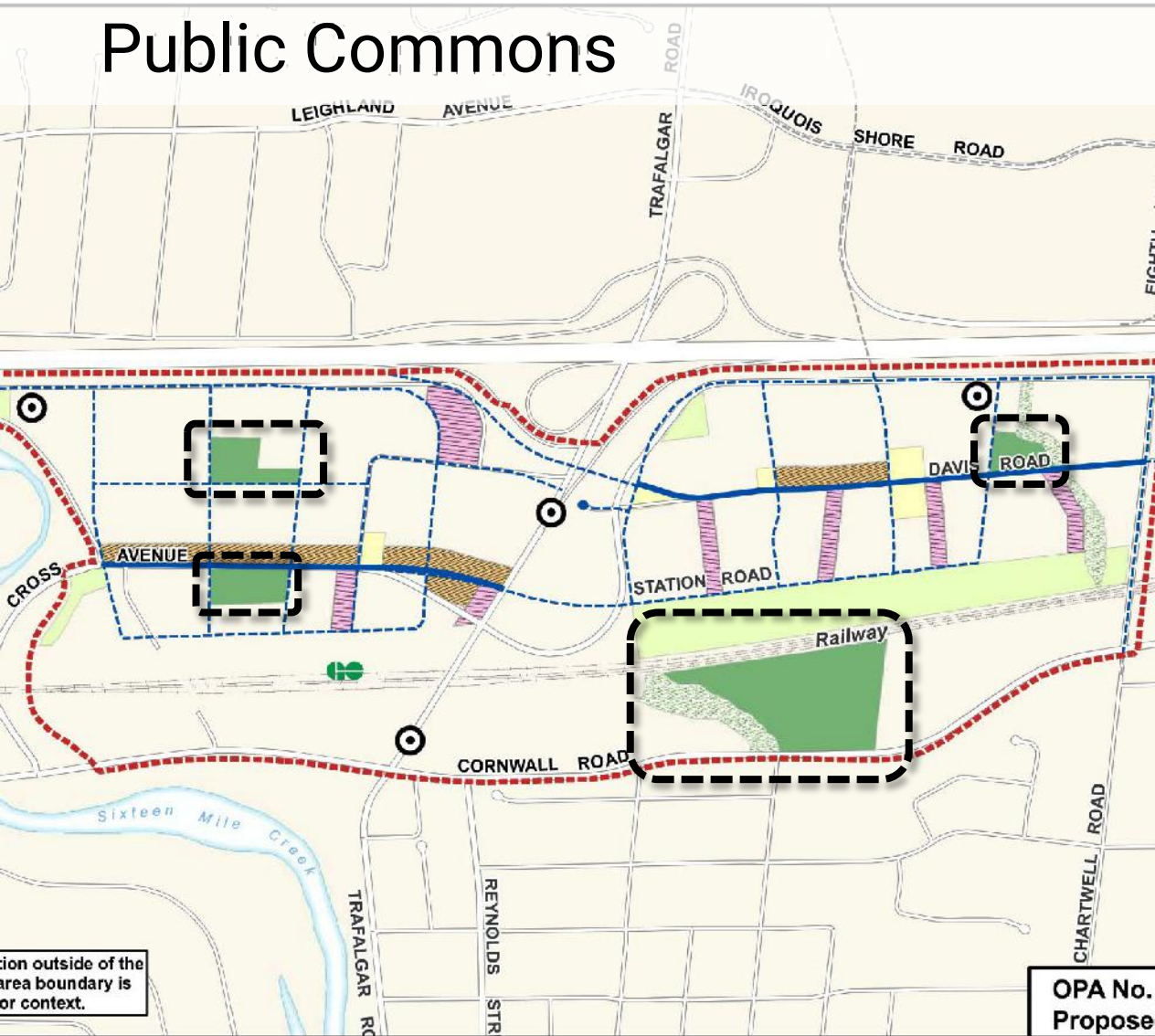
# Public Realm: Proposed Schedule L4

## Urban Squares



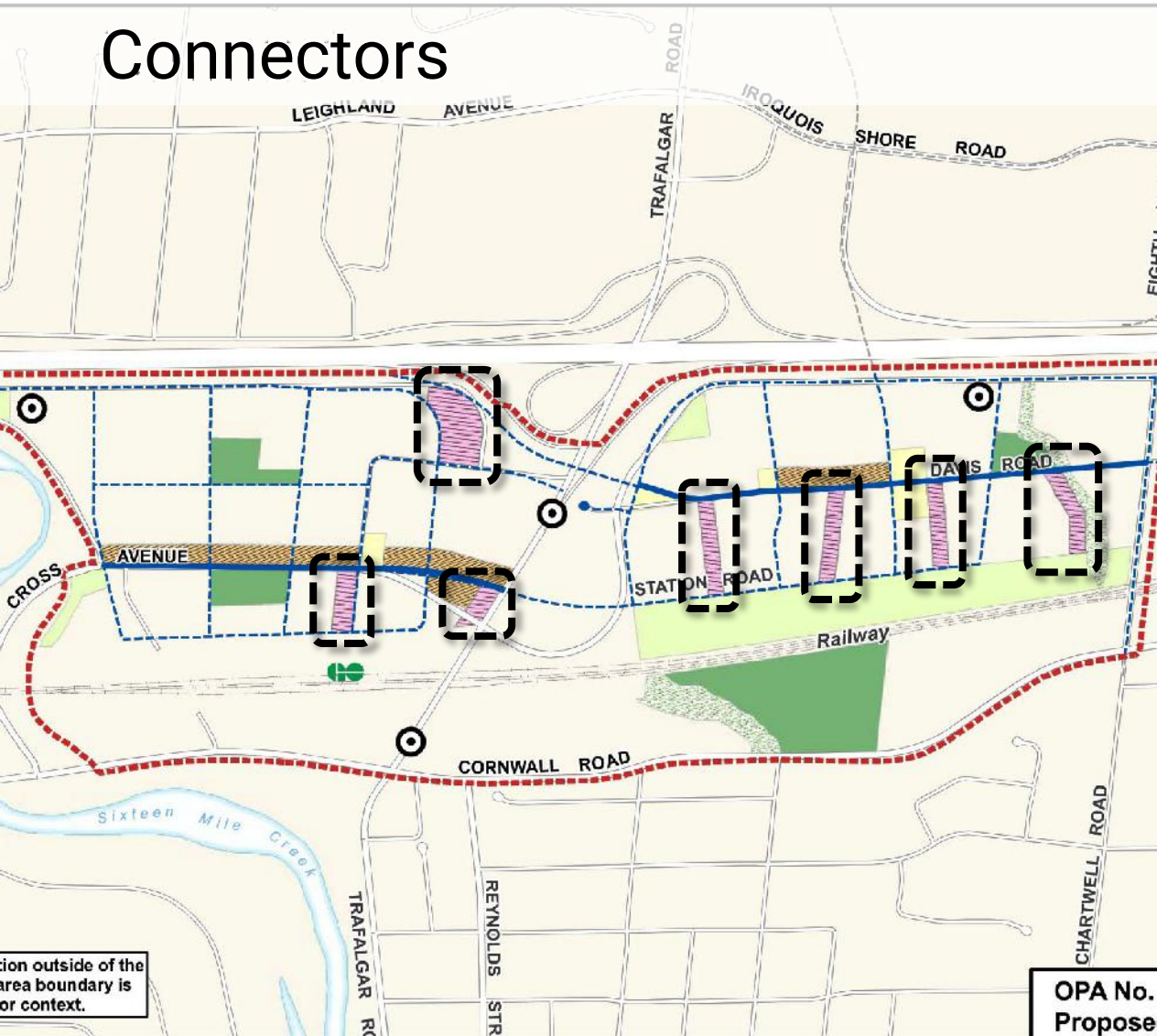
# Public Realm: Proposed Schedule L4

## Public Commons



# Public Realm: Proposed Schedule L4

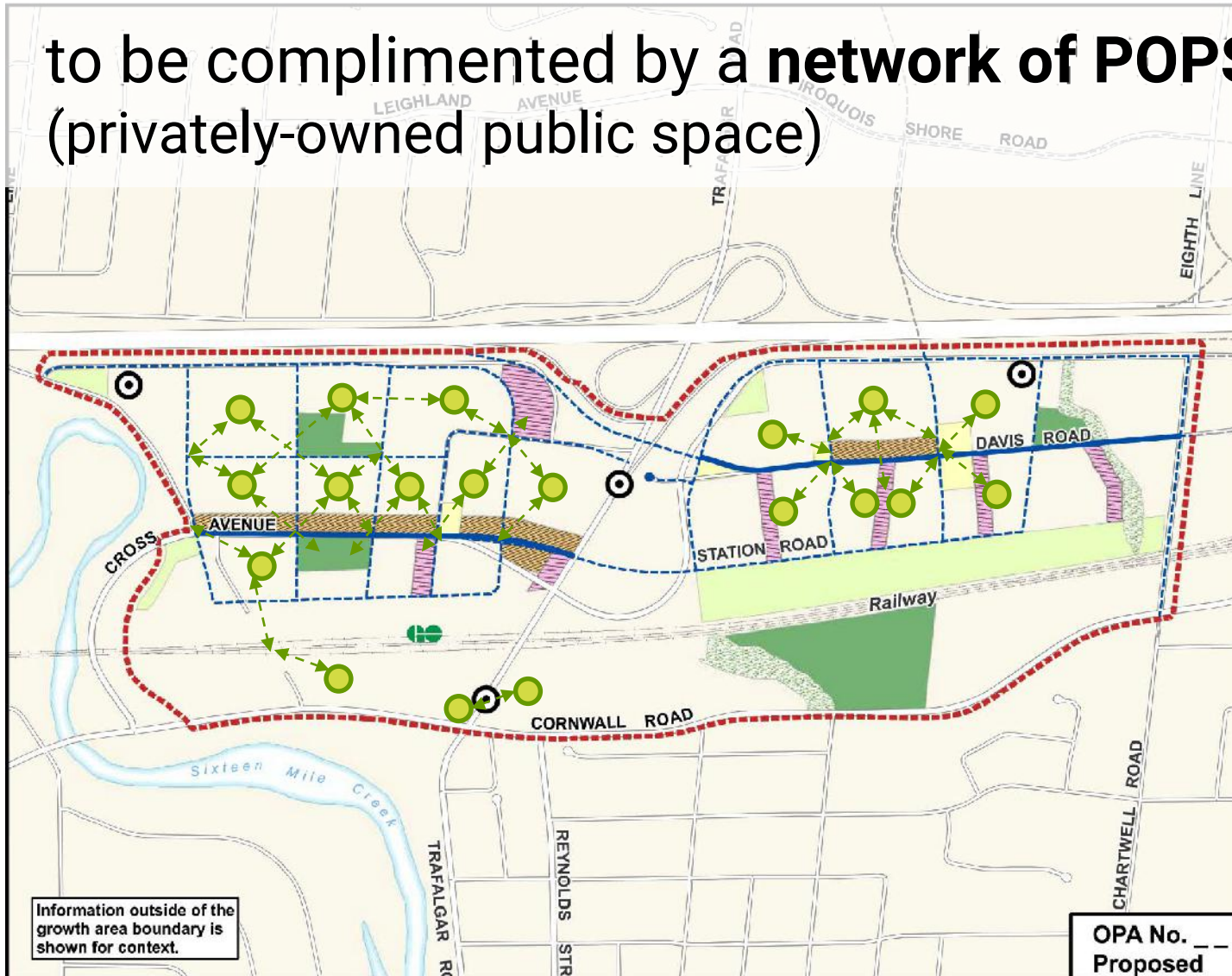
## Connectors





# Public Realm: Proposed Schedule L4

to be complimented by a **network of POPS**  
(privately-owned public space)



**SCHEDULE L4  
MIDTOWN OAKVILLE  
PUBLIC REALM**

- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
- MAIN STREET
- GATEWAY
- PROMENADE
- URBAN SQUARE
- PUBLIC COMMON
- CONNECTOR
- OPEN SPACE
- NATURAL AREA
- FUTURE ROADS (NOT TO BE CONSIDERED)
- RAILWAY
- MAJOR TRANSIT STATION

REFER TO PART B, MIDTOWN OAKVILLE FOR GROWTH AREA POLICIES

1:6,500  
MAY 12, 2022

OPA No. --  
Proposed

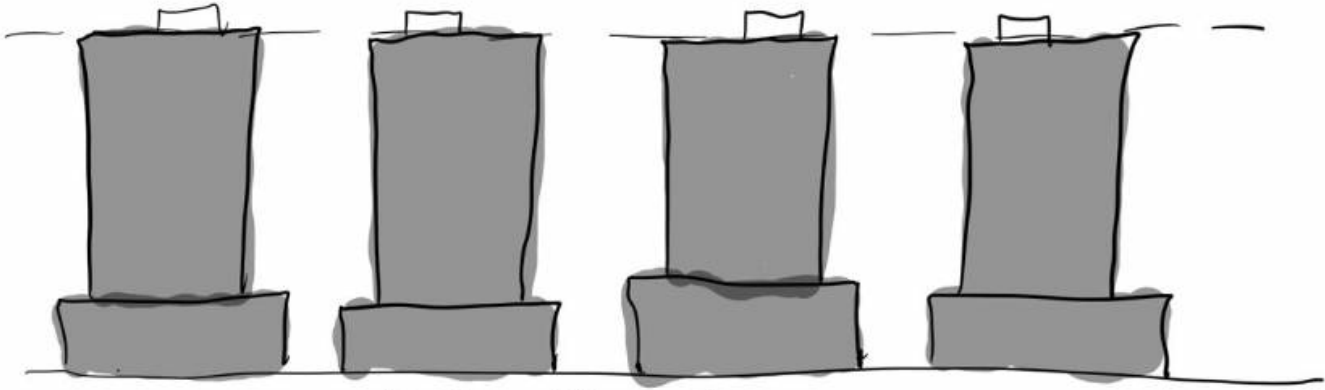
SCHEMATIC REPRESENTATION OF LANDSCAPE ARCHITECTURE FOR THE PUBLIC REALM OF MIDTOWN OAKVILLE. THIS IS A PROPOSED PLAN AND NOT A CONTRACT DOCUMENT.

Information outside of the growth area boundary is shown for context.

# POPS Example: Proposed

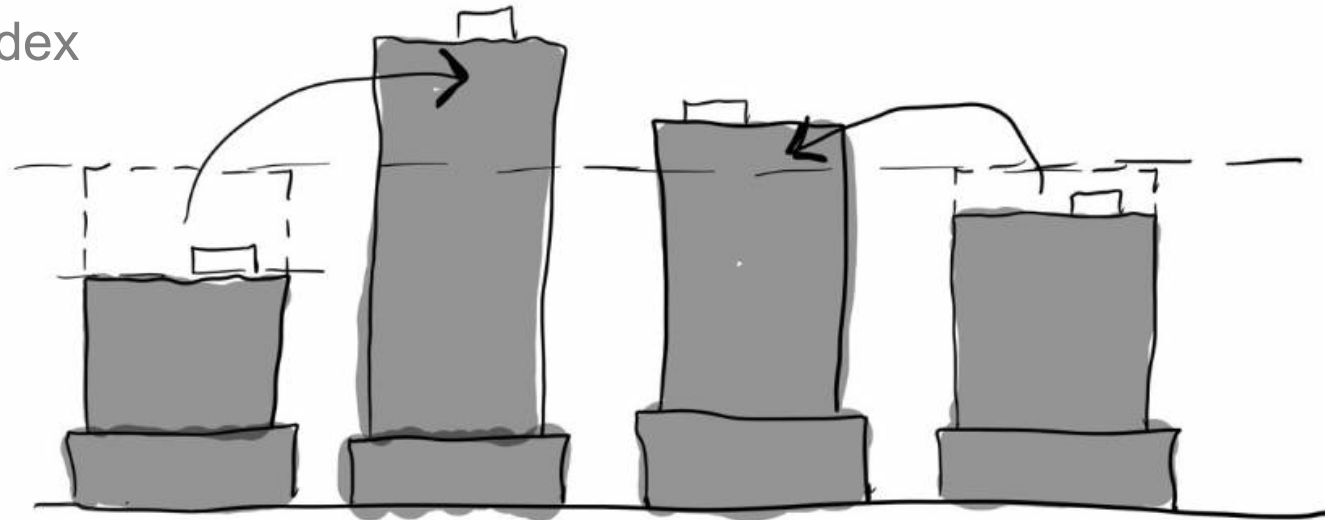


# From Building Heights to Floor Space Index



**Building Height Maximum**

FSI = Floor  
Space Index



**FSI Maximum and Building Height Variation**

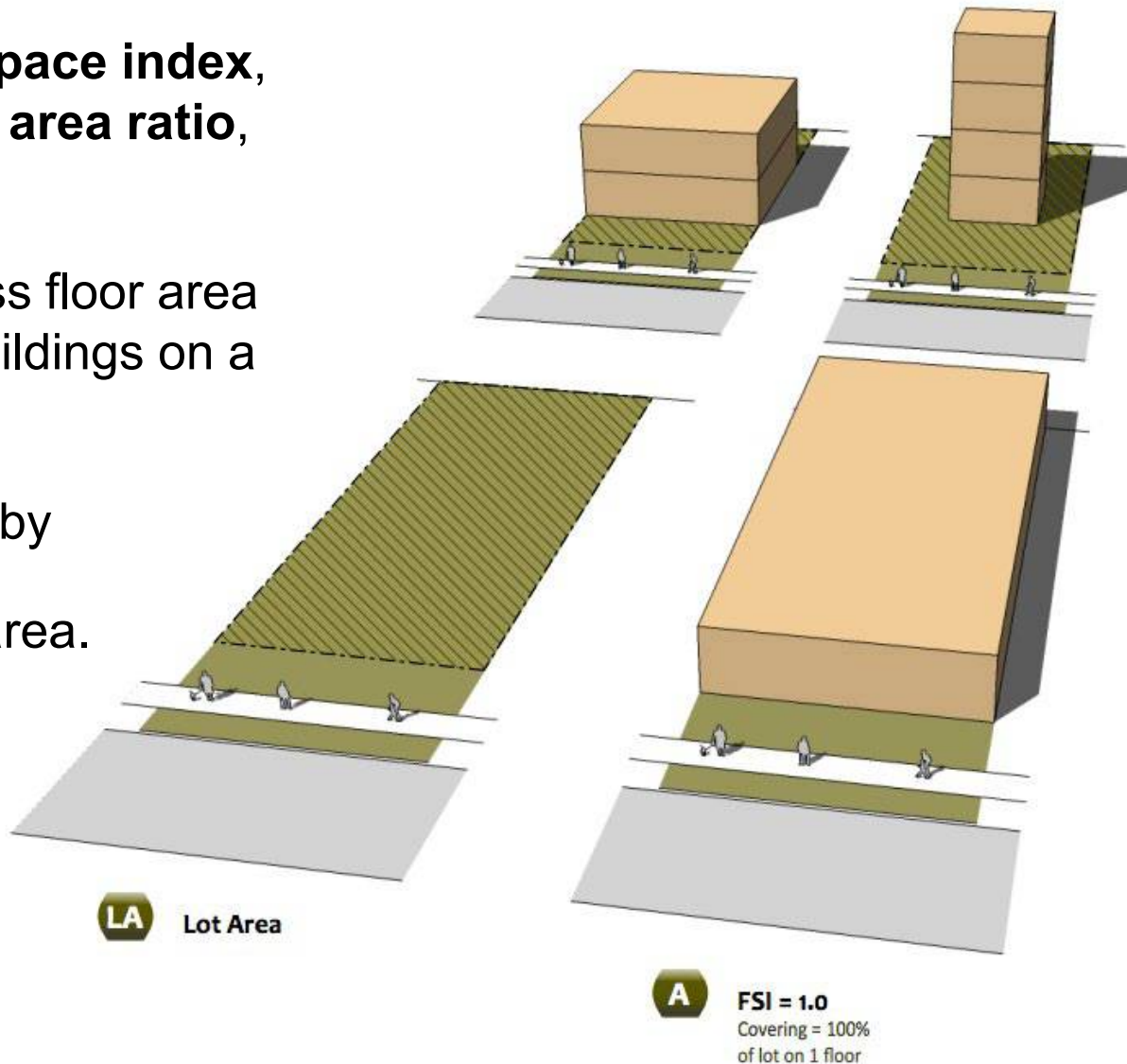
# Floor Space Index

**Floor space index, or floor area ratio, means**

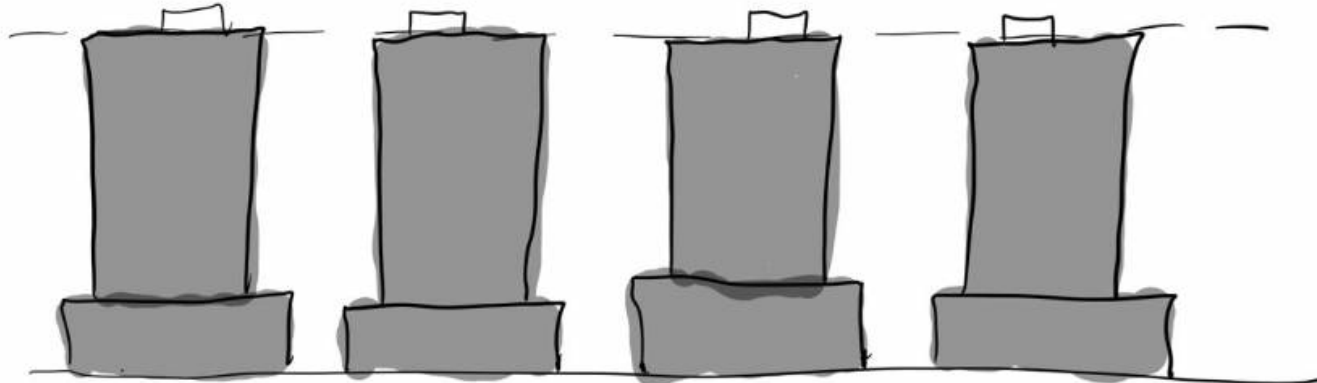
the gross floor area of all buildings on a lot

divided by

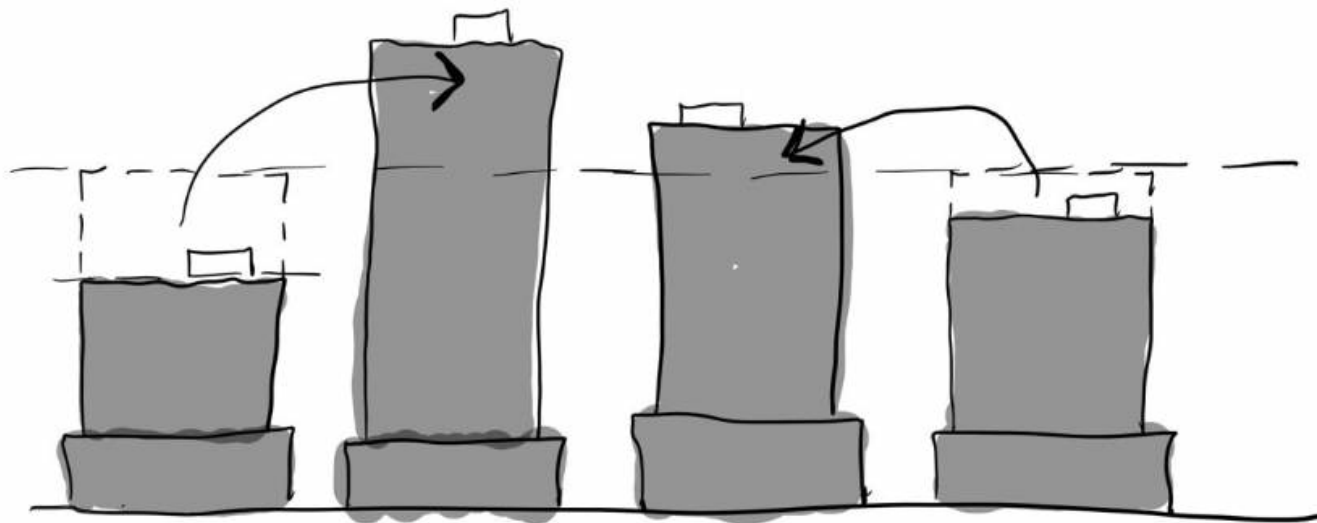
the lot area.



# From Building Heights to Floor Space Index



**Building Height Maximum**



**FSI Maximum and Building Height Variation**

# Determining Minimum FSI

**Midtown Oakville has a GROSS area of 103 ha.**

An aerial photograph of the Midtown Oakville area, outlined in red. The text "103 hectares" is overlaid in red on the map. The map shows a mix of residential, commercial, and green spaces, with a large road and a river visible.

**103 hectares**

# Determining Minimum FSI

**200** people and jobs per hectare x **103** ha  
= **20,600** people and jobs MINIMUM



**103 hectares**

# Determining Minimum FSI

**Not all** of this **GROSS area** can be redeveloped to accommodate **people and jobs**.

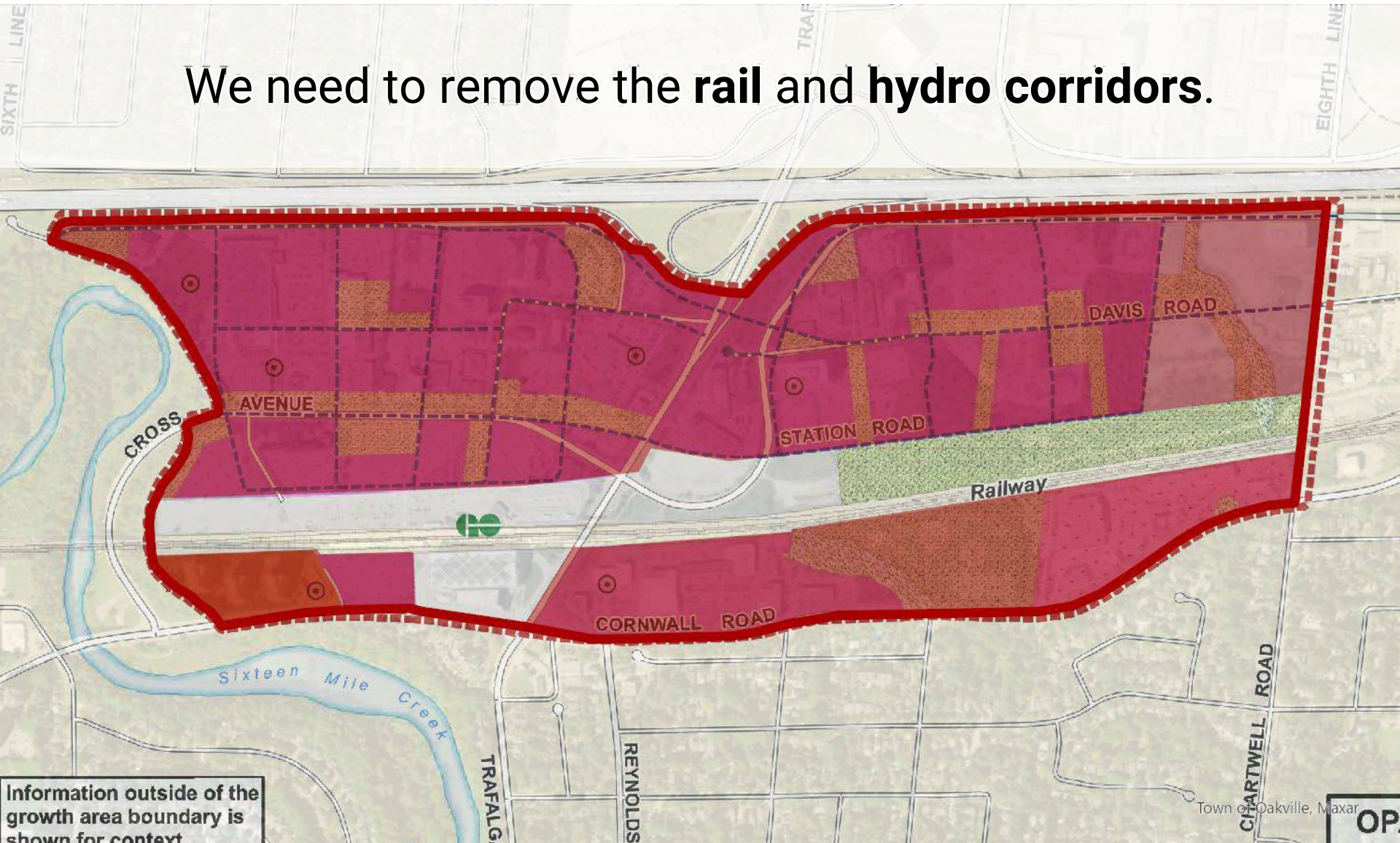


103 hectares



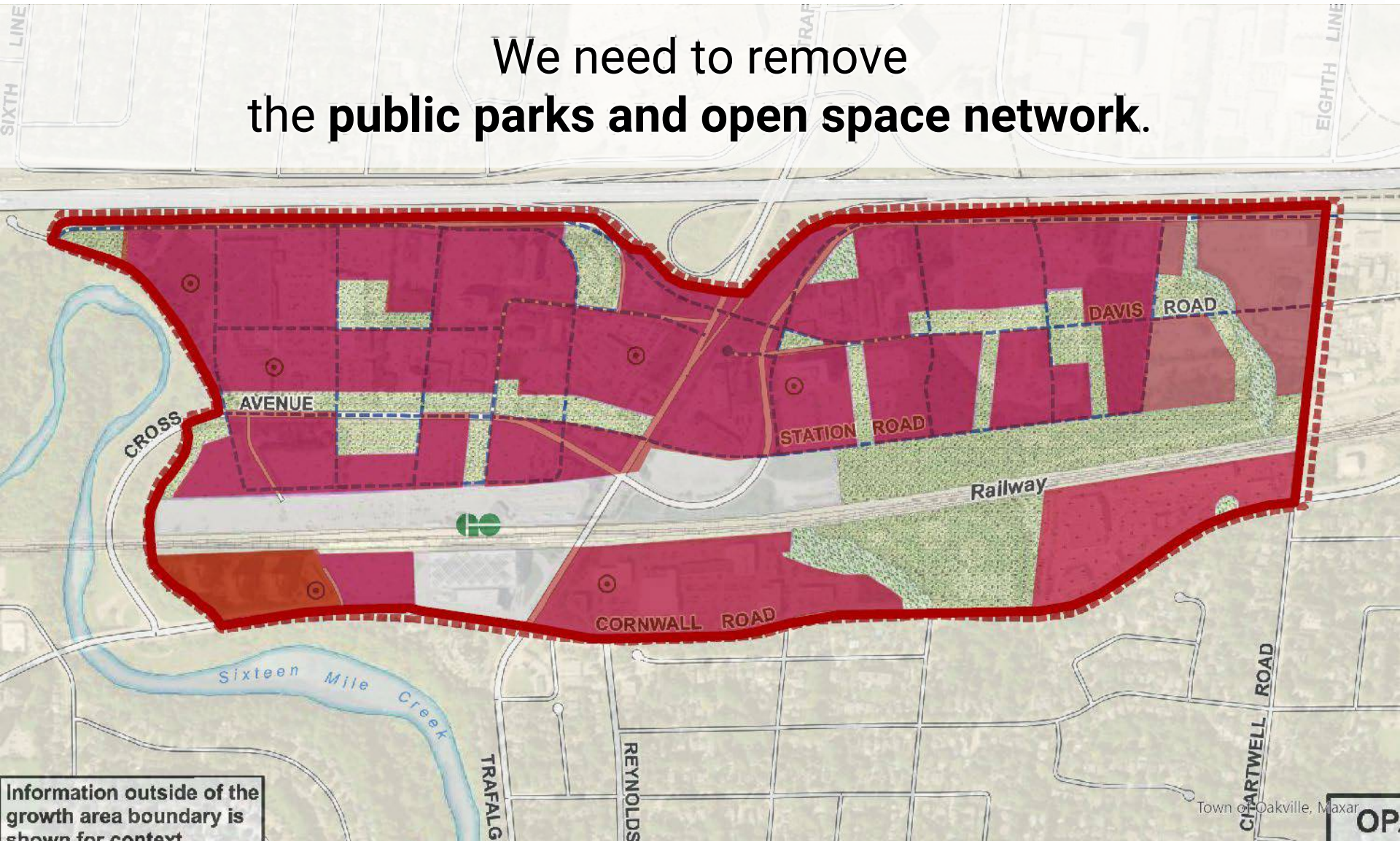
# Determining Minimum FSI

We need to remove the **rail** and **hydro corridors**.



# Determining Minimum FSI

We need to remove  
the **public parks and open space network.**



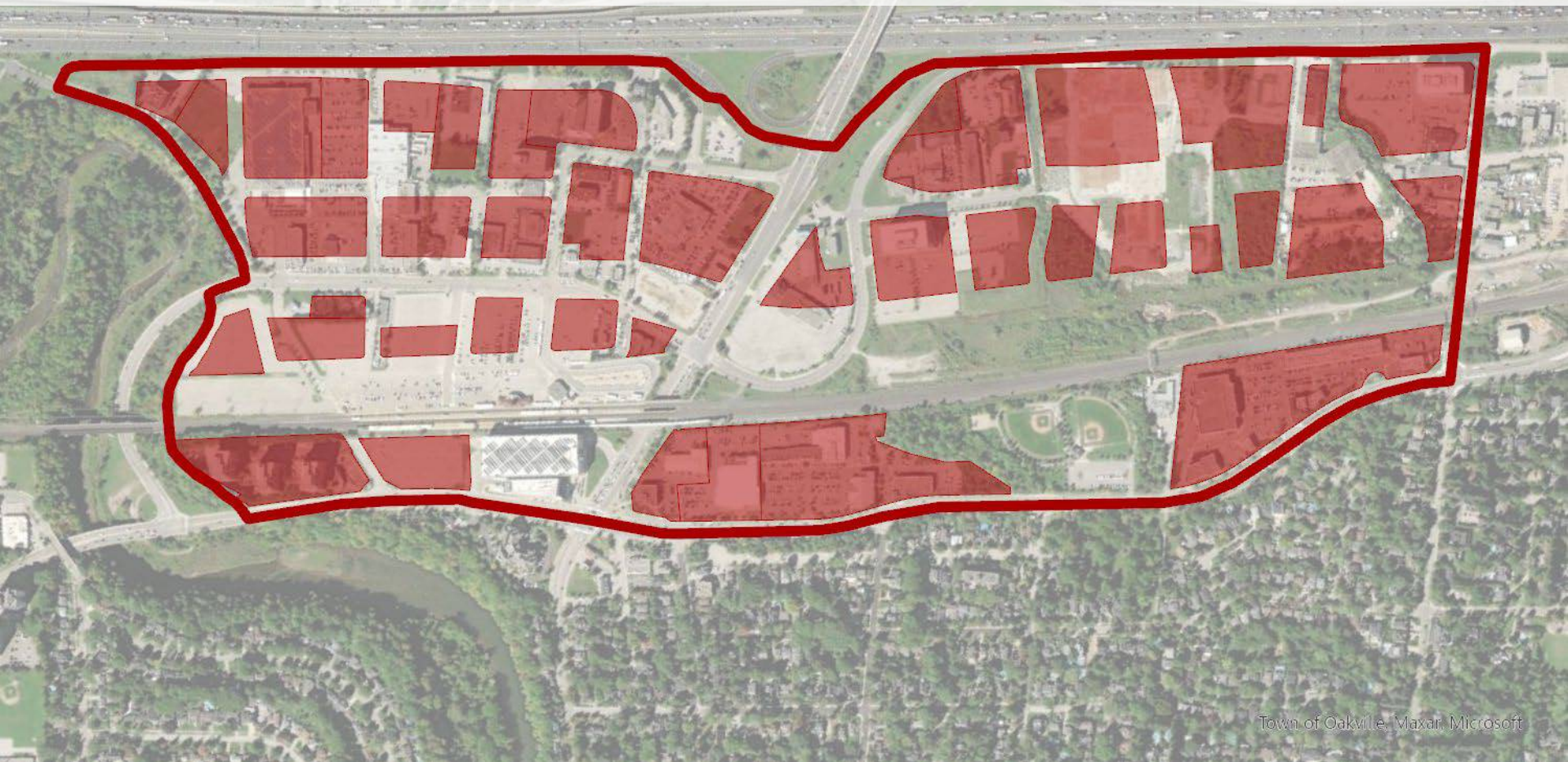
# Determining Minimum FSI

We need to remove the **street network**.



# Determining Minimum FSI

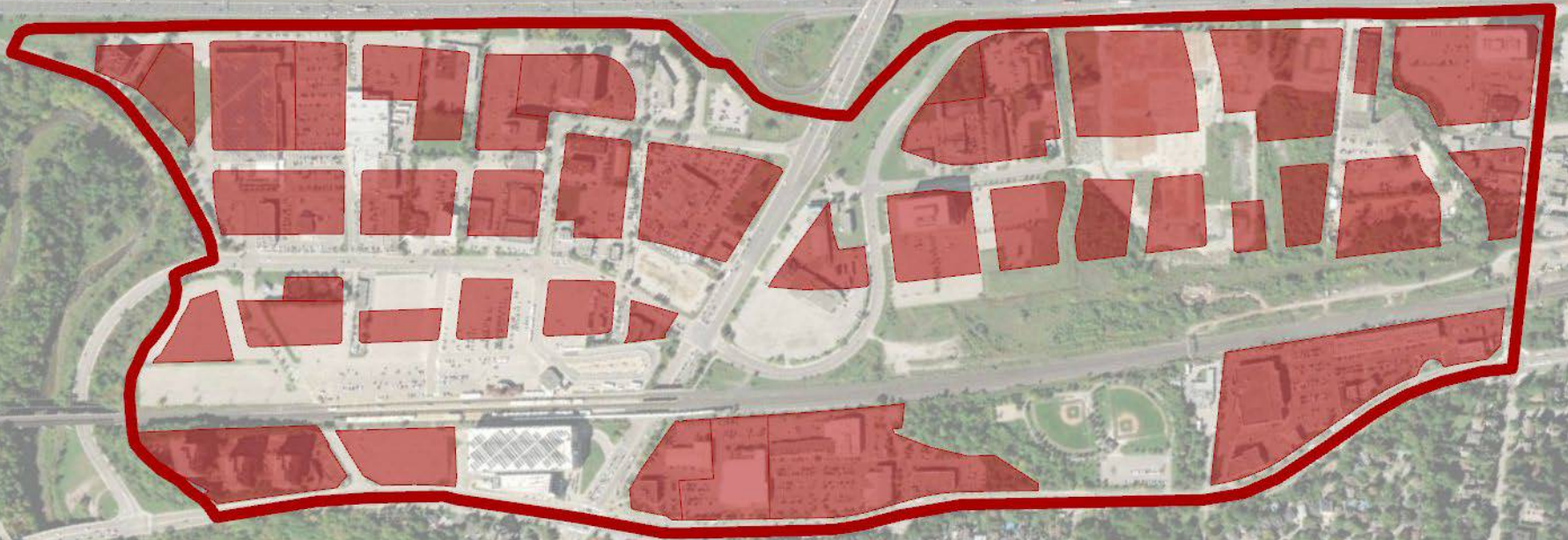
**We are left with the parcels that can be redeveloped.**



Town of Oakville, Maxar, Microsoft

# Determining Minimum FSI

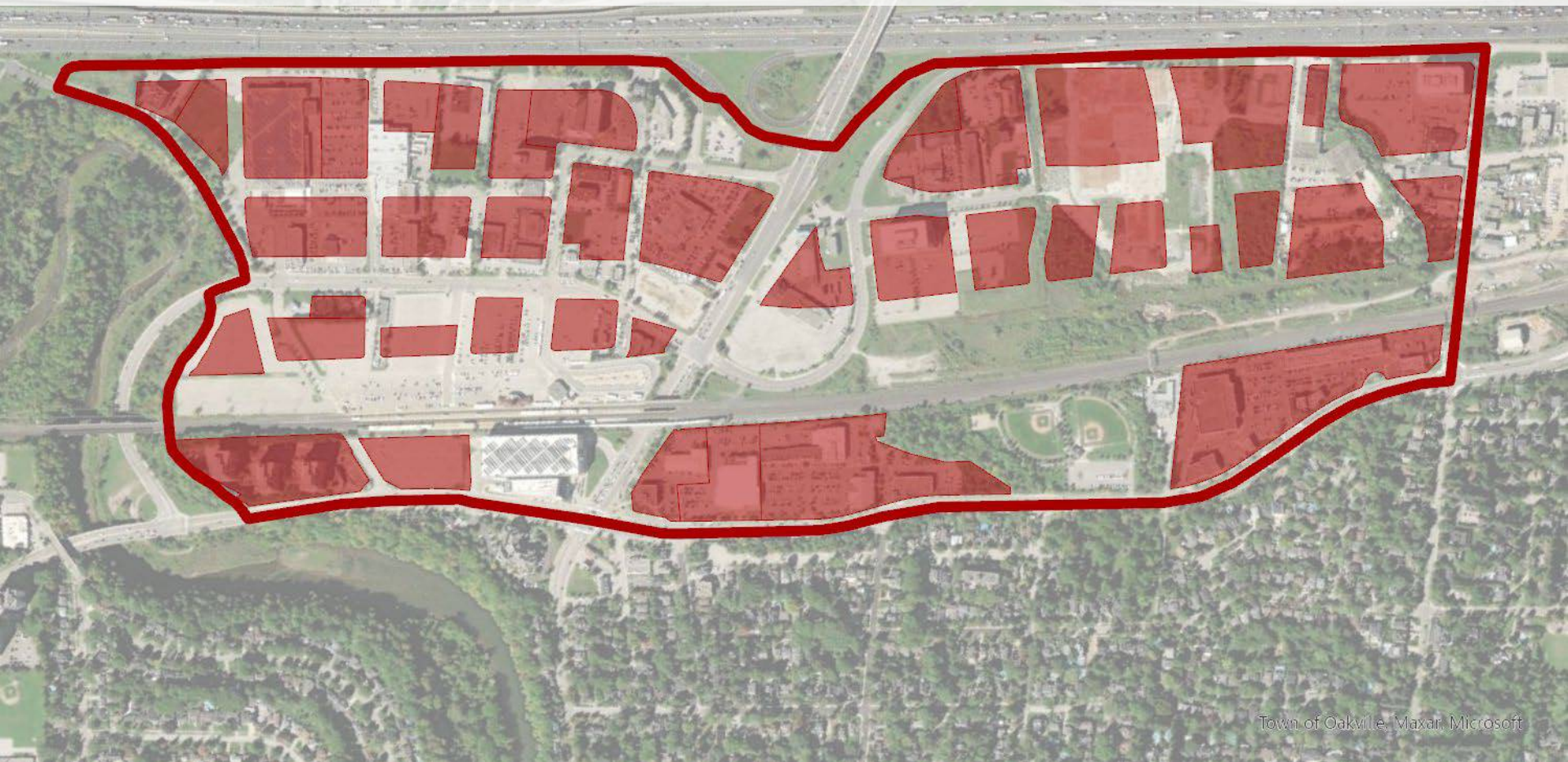
**43 hectares**  
(or ~40% of the GROSS 103 ha)



Town of Oakville, Maxar, Microsoft

# Determining Minimum FSI

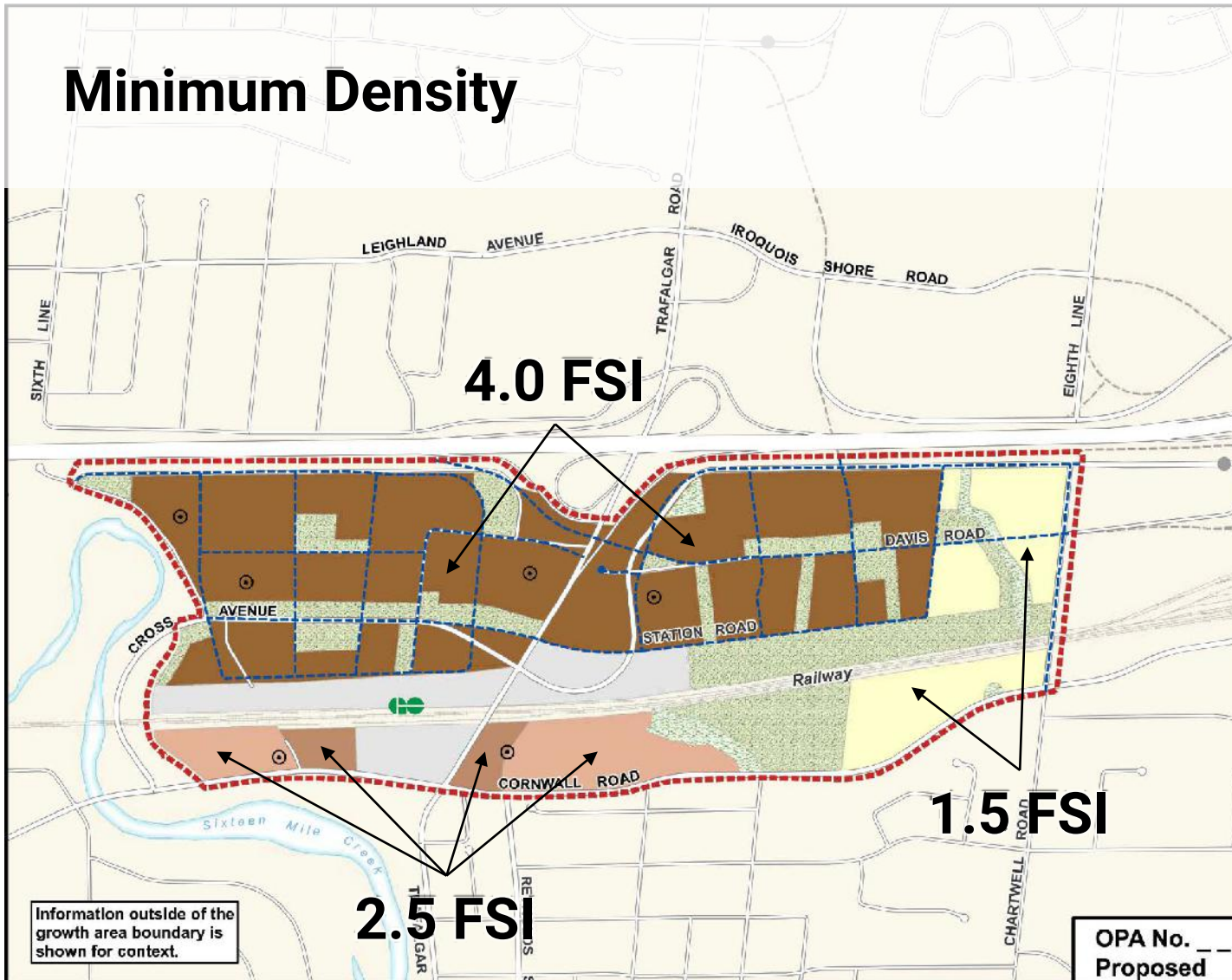
The minimum 20,600 people and jobs, on 43 net hectares  
= ~ 480 people and jobs/net hectare



Town of Oakville, Maxar, Microsoft

# Density: Proposed Schedule L2

## Minimum Density



Information outside of the growth area boundary is shown for context.

OPA No. \_\_\_  
Proposed

### SCHEDULE L2 MIDTOWN OAKVILLE DENSITY



- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
- 1.5 TO 4 FSI
- 2.5 TO 4 FSI
- 2.5 TO 6 FSI
- 4.0 TO 10 FSI \*
- NATURAL AREA
- PARKS AND OPEN SPACE
- DENSITY COMPATIBLE WITH UTILITY CORRIDOR
- FUTURE ROAD'S (RIGHT OF WAY - 30 METRE WIDE BUFFER)
- FUTURE CURVE S.V.C.
- RAILWAY
- MAJOR TRANSIT STATION
- REFER TO PART E: MIDTOWN OAKVILLE EXCEPTIONS
- \* ADDITIONAL DENSITY MAY BE PERMITTED, REFER TO PART E: MIDTOWN OAKVILLE FOR GROWTH MANAGEMENT



1:7,500  
MAY 12, 2022

SOURCE: MIDTOWN OAKVILLE DEVELOPMENT PLAN 2022-2026, PART E: MIDTOWN OAKVILLE FOR GROWTH MANAGEMENT

# Determining Maximum FSI

**Not ALL parcels will redevelop in the short-term.**

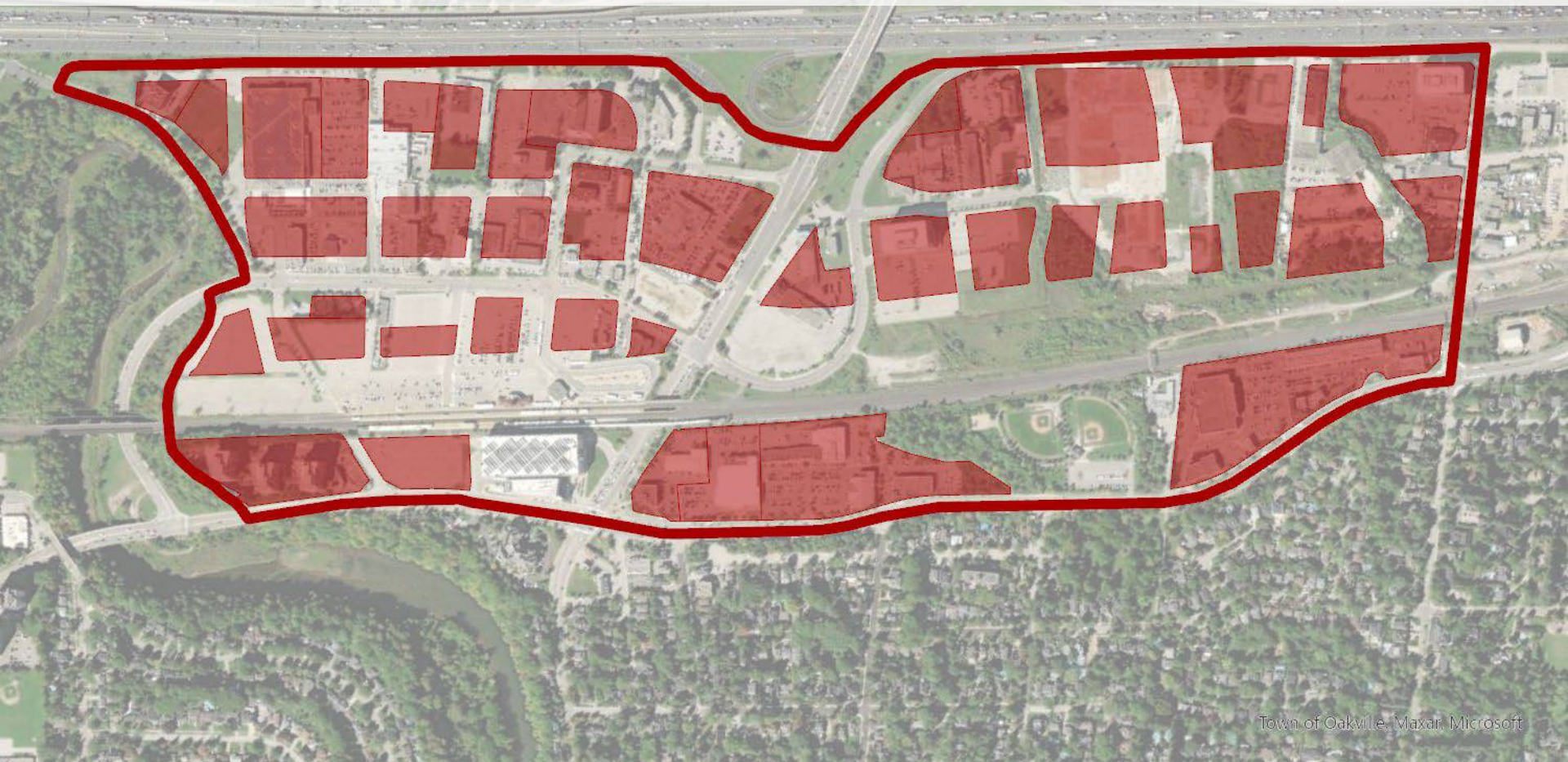


Town of Oakville, Maxar, Microsoft



# Determining Maximum FSI

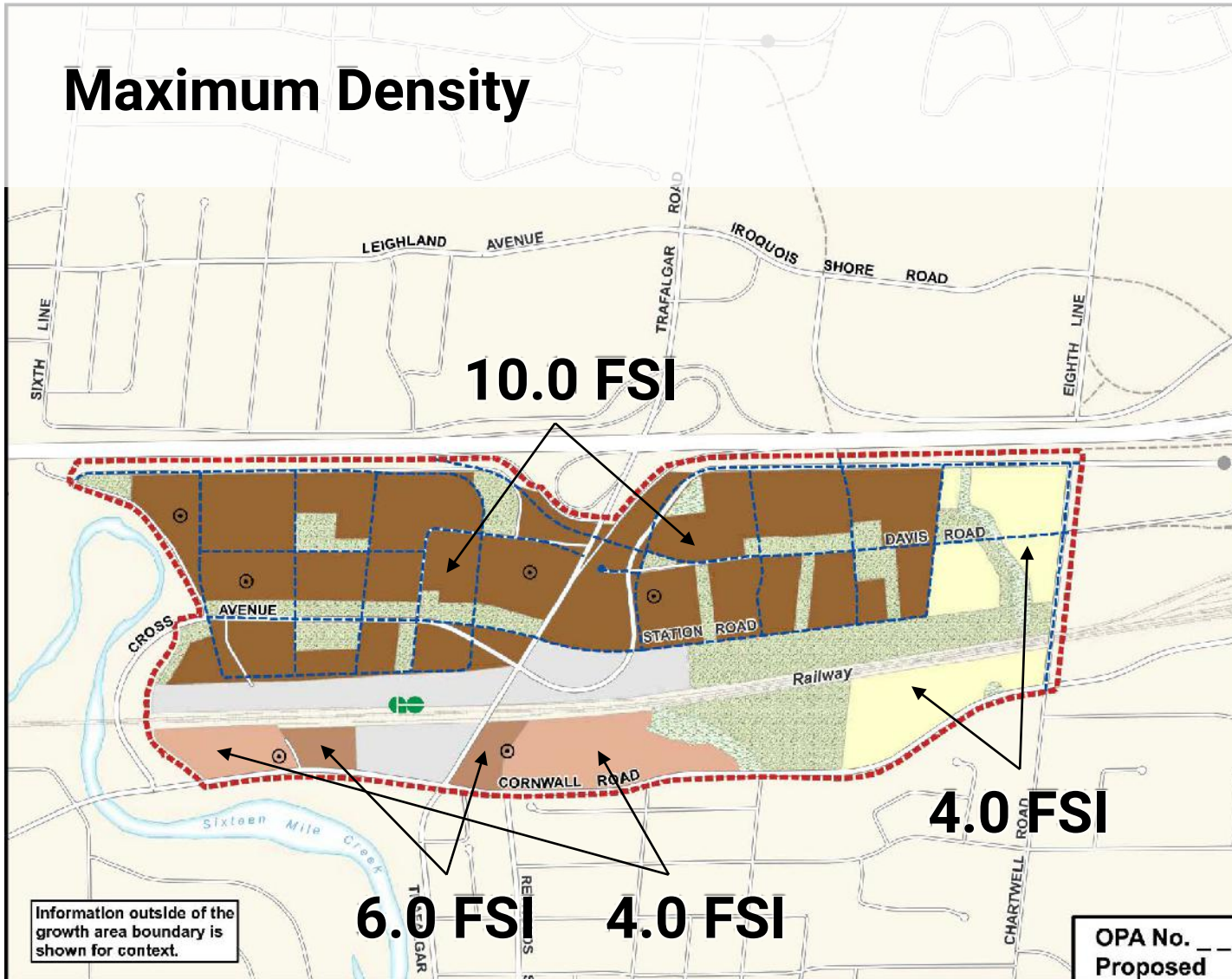
**Applying a maximum FSI allows for the Town to incent elements that might not otherwise happen on their own.**



Town of Oakville, Maxar, Microsoft

# Density: Proposed Schedule L2

## Maximum Density



Information outside of the growth area boundary is shown for context.

OPA No. --  
Proposed

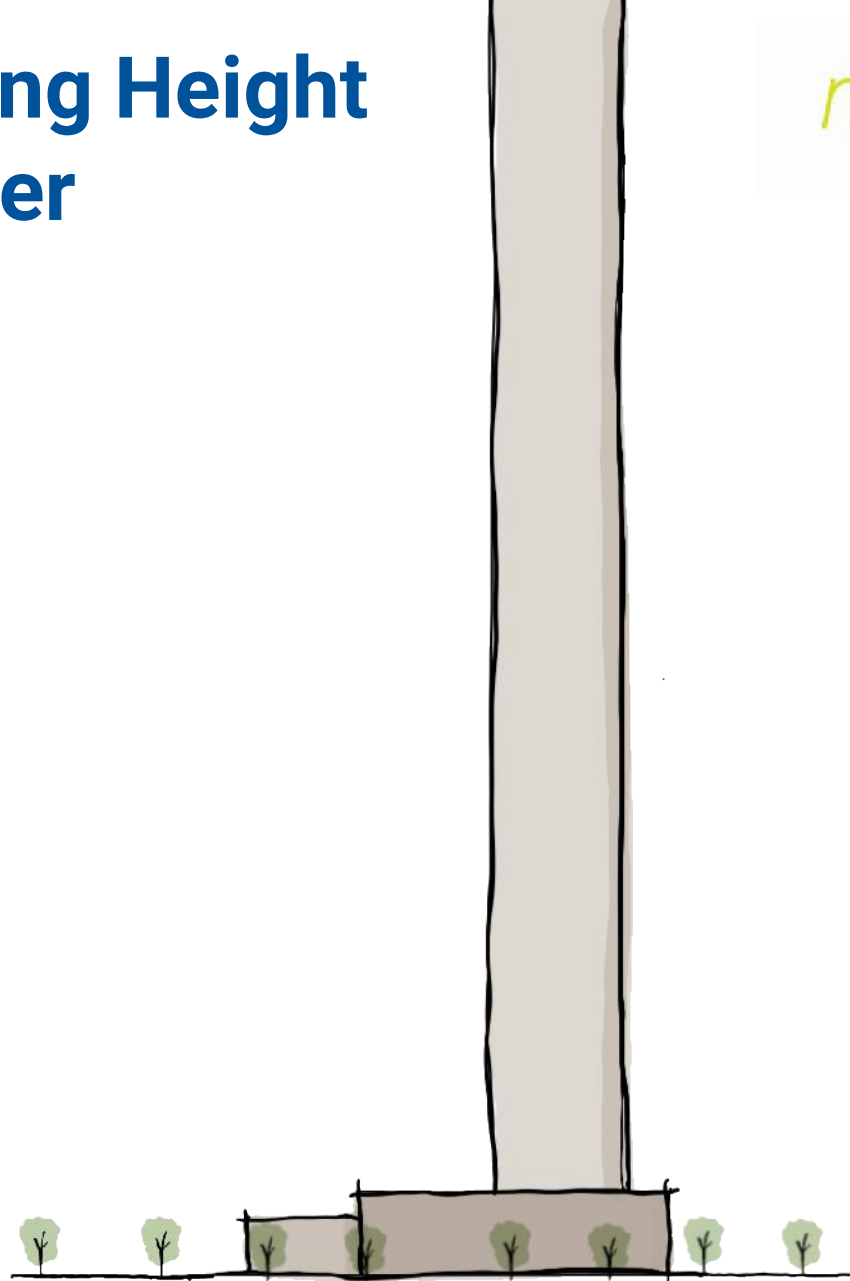
### SCHEDULE L2 MIDTOWN OAKVILLE DENSITY

- URBAN GROWTH CENTRE / PROTECTED MAJOR TRANSIT STATION AREA / GROWTH AREA BOUNDARY
- 1.8 TO 4 FSI
- 2.5 TO 4 FSI
- 2.5 TO 6 FSI
- 4.0 TO 10 FSI \*
- NATURAL AREA
- PARKS AND OPEN SPACE
- DENSITY COMPATIBLE WITH UTILITY CORRIDOR
- FUTURE ROAD/S (GREEN TO BROWN, 3.0 TO 10.0 FSI)
- FUTURE COLLEGE SAC
- RAILWAY
- MAJOR TRANSIT STATION
- REFER TO PART E, MIDTOWN OAKVILLE EXCEPTIONS
- \* ADDITIONAL DENSITY MAY BE PERMITTED, REFER TO PART E, MIDTOWN OAKVILLE, FOR GROWTH MANAGEMENT

1:7,500  
MAY 12, 2022

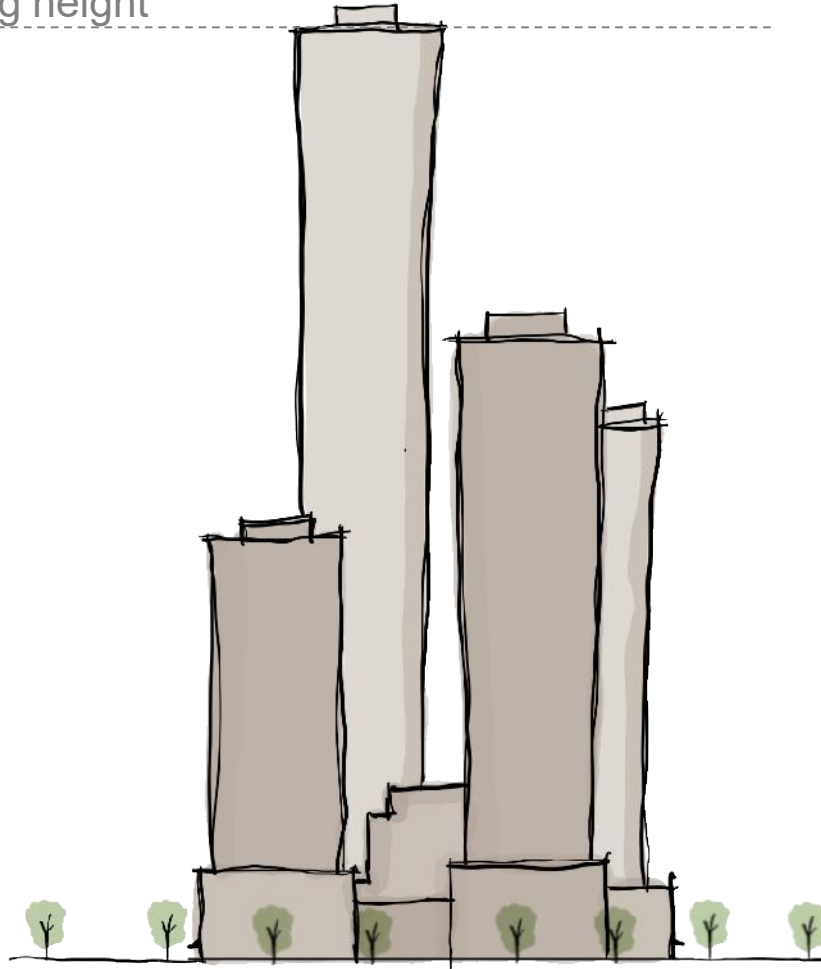
SOURCE: MIDTOWN OAKVILLE, 2022. DRAWN BY: [unreadable]

# Maximum Building Height for a Single Tower

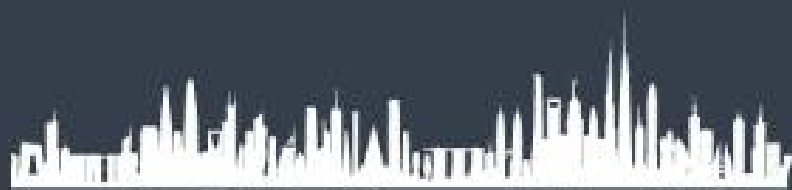


# Maximum Building Height for a Single Tower

maximum building height



# Building Height Calculator



## HEIGHT CALCULATOR

Function ⓘ

Mixed Use: Office & Residential



At least 15% of total gross floor area (GFA) or building height each

Floors Above Ground ⓘ

Generate Height

Height (m) ⓘ

255

Generate Floor Count

Floors:

**65**

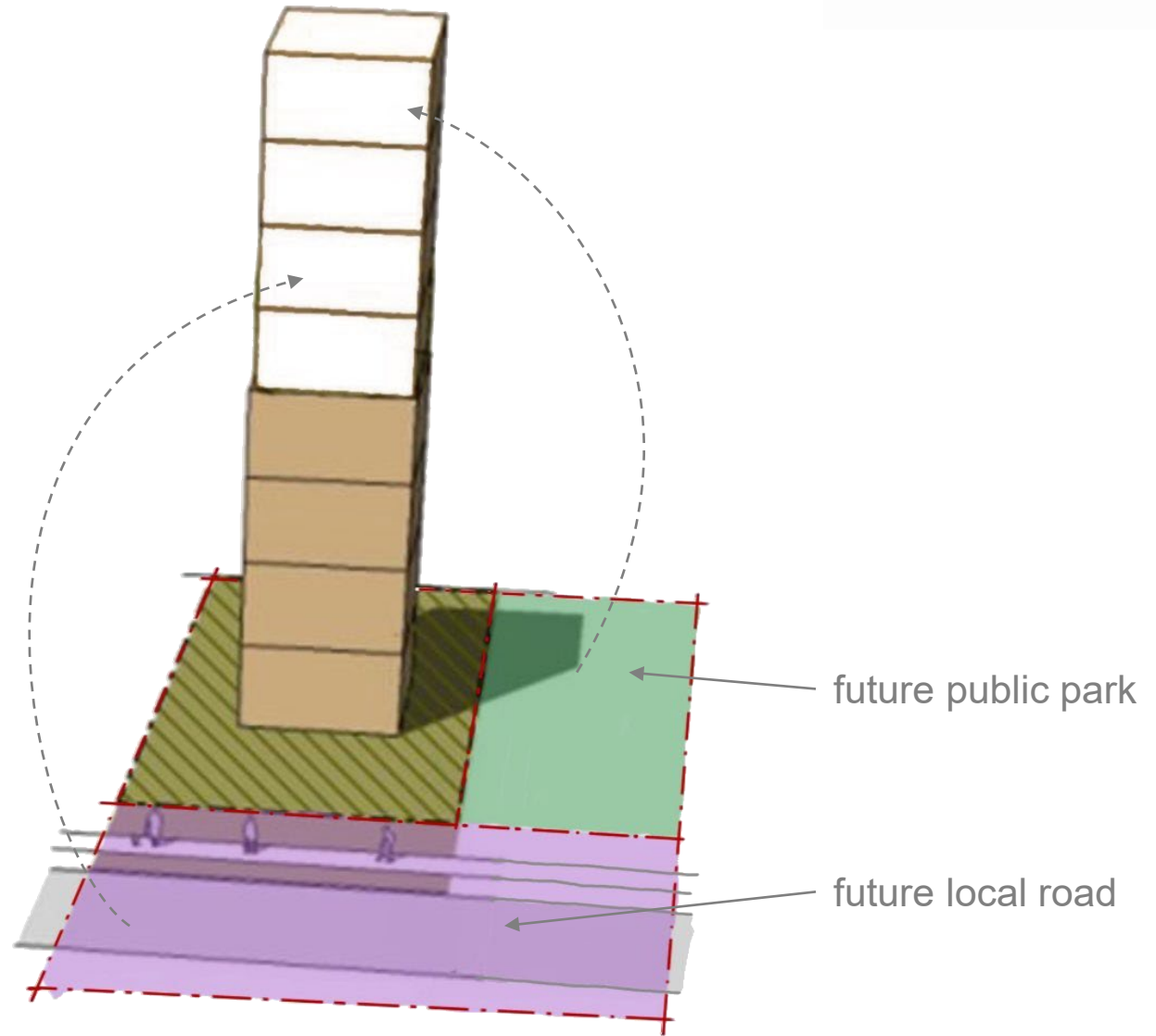
# Example: Development Proposal



# FSI as a planning tool

- Obtaining land for **public parks** and **future local roads** is very important in order to achieve this vision for Midtown.
- Proposed policies allow for landowners to **retain the development potential** of land provided to the Town for these purposes.

# Retain FSI from Public Parks and Roads





# FSI as a planning tool

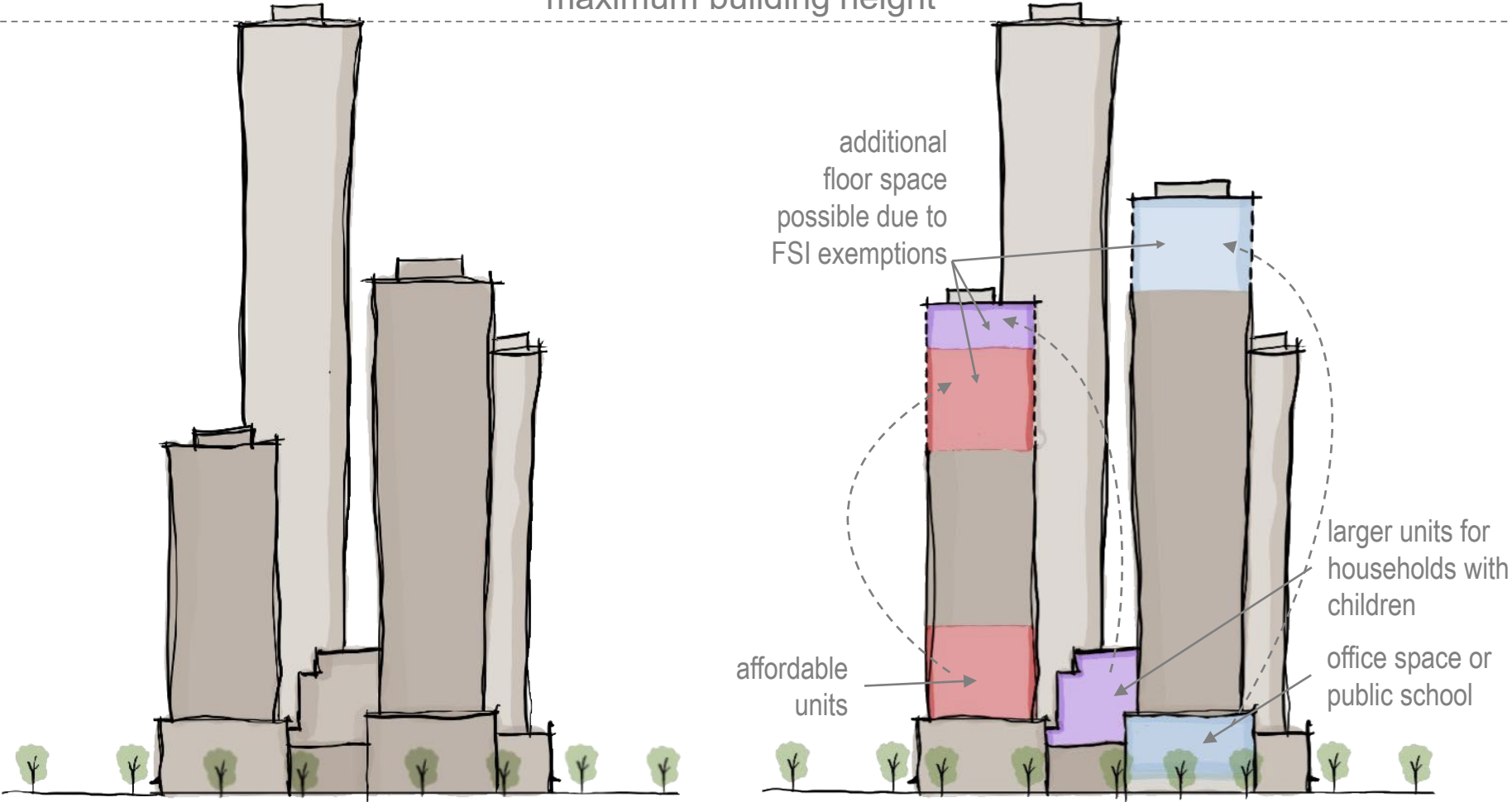
- Proposed policies offer **FSI exemptions** when developers provide desired components as part of development.
- The draft OPA offers exemptions for:
  - Affordable units
  - Office space in mixed use buildings
  - Housing for households with children
  - Public schools, when integrated into mixed use buildings
  - Bicycle parking and maintenance areas

# FSI as a planning tool

- **FSI exemptions for future consideration:**
  - larger grocery stores
  - daycares
  - hotels
  - above-ground structured parking capable of being converted to other uses in the future
  - others

# Additional Floor Area based on FSI Exemptions

maximum building height



# Inclusionary Zoning

- Inclusionary Zoning (IZ) is a tool to **require** the inclusion of **affordable housing units** within buildings.
- Units will be **maintained as affordable** housing units **over time**.
- The *Planning Act* inclusionary zoning for **protected major transit station areas** like Midtown.
- Town Council must first adopt **enabling** general official plan **policies**.
- Those policies need to be based on an **assessment report** (by either the Town or Region).

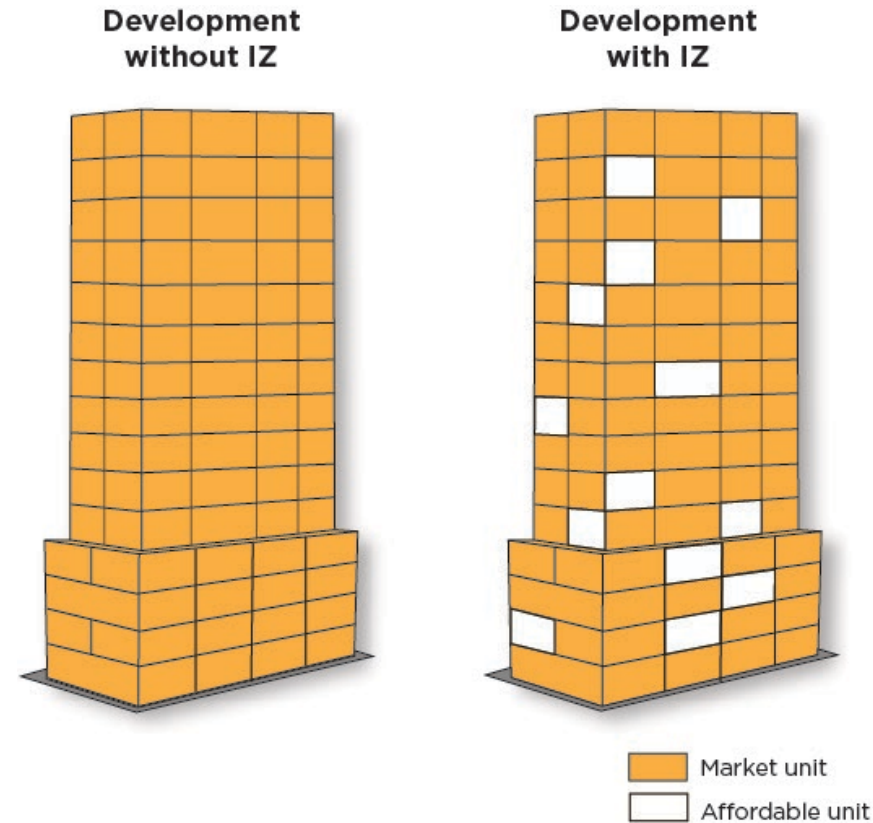


Image Source: City of Mississauga

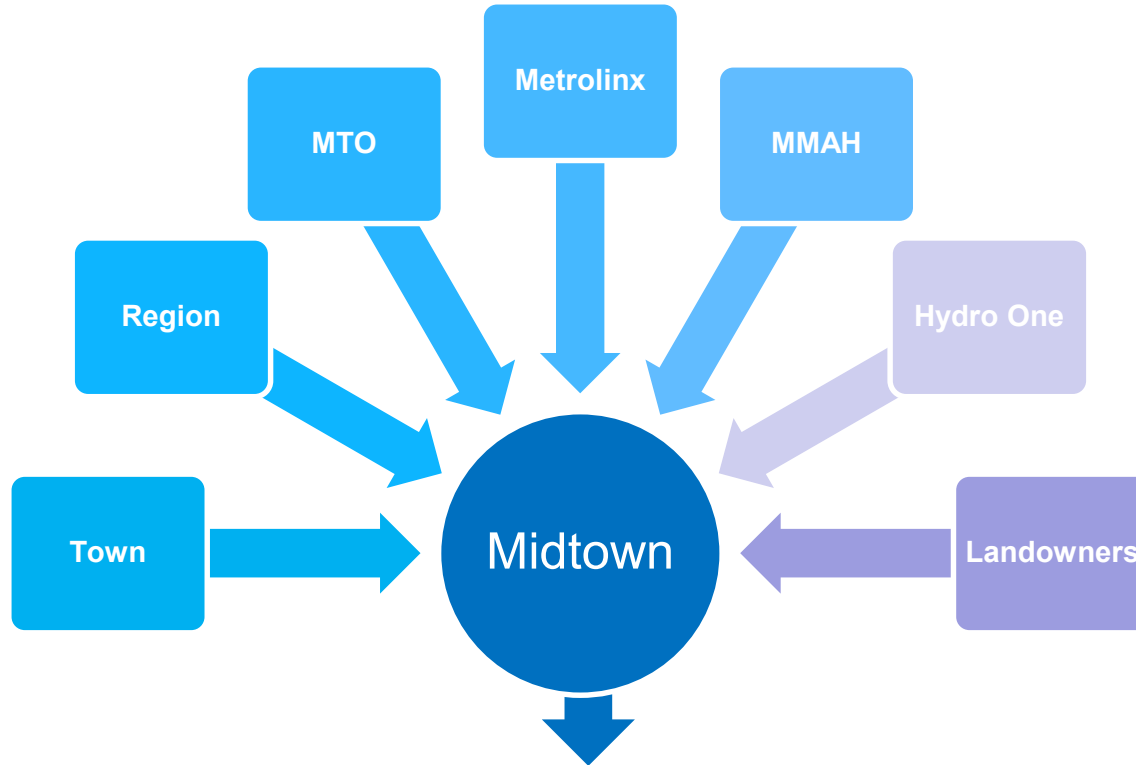
# Coordination and Cost-sharing

- This OPA **does not have** the previously proposed **requirement for landowner agreements.**
- Stakeholder feedback indicated that **tools are already available to the Town** for development coordination and cost-sharing purposes.

# Coordination and Cost-sharing

- What are the **tools** the Town will use to coordinate development and facilitate cost-sharing?
  - **Development Charges**
  - **Community Benefit Charge**
  - **Parkland Dedication** (land and cash)
  - **Local Service Policy**
  - Midtown **Area-specific Development Charges**
- **Area Design Plans / Block Plans**,  
at the Town's discretion

# Implementation Strategy



***Moving forward together***

# Implementation Strategy

- Implementation strategies needed in Midtown:
  - a **parkland** strategy
  - **transportation** initiatives — including transit and active transportation
  - **streetscape** and/or **public realm** plans
  - **parking** strategy
  - **extension of the rail platform** across Trafalgar Road
  - **community improvement plan**
  - sustainability initiatives such as **district energy**, **geothermal** heating/cooling and other utilities
  - **public sector partnerships** and programs
  - the municipal **acquisition and disposal of land**
  - **servicing** strategy, implementation and phasing

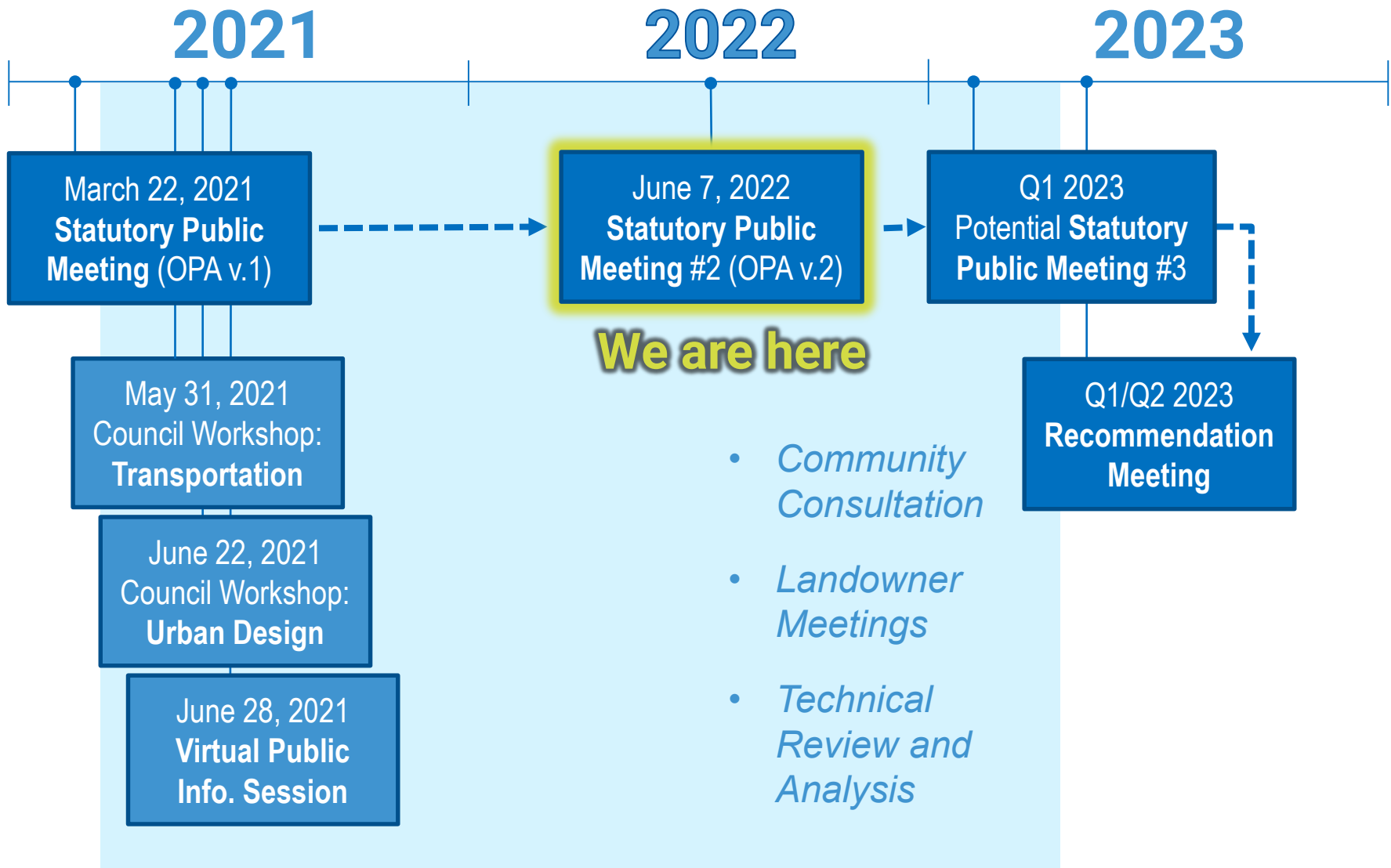


# Implementation Strategy



**Midtown Oakville** will be a high density urban area that is **unique** from the typical lower density development of Oakville to date and **will require a different way** of delivering the pieces needed to build Midtown Oakville as envisioned.

# OPA Timeline / Next Steps



# Recommendation

1. That the staff report titled “Public Meeting Report – Town-initiated Official Plan Amendment – Midtown Oakville Urban Growth Centre (File No. 42.15.59) – June 7, 2022” from the Planning Services Department be received.
2. That comments from Council and the public with respect to the draft town-initiated official plan amendment to implement the findings of the Midtown Oakville Growth Area Review (File No. 42.15.59) be received.

